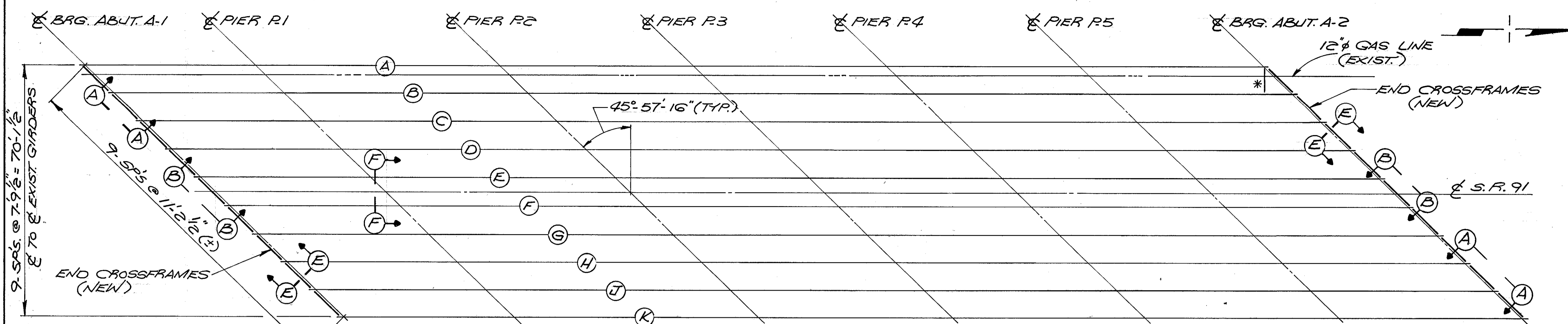
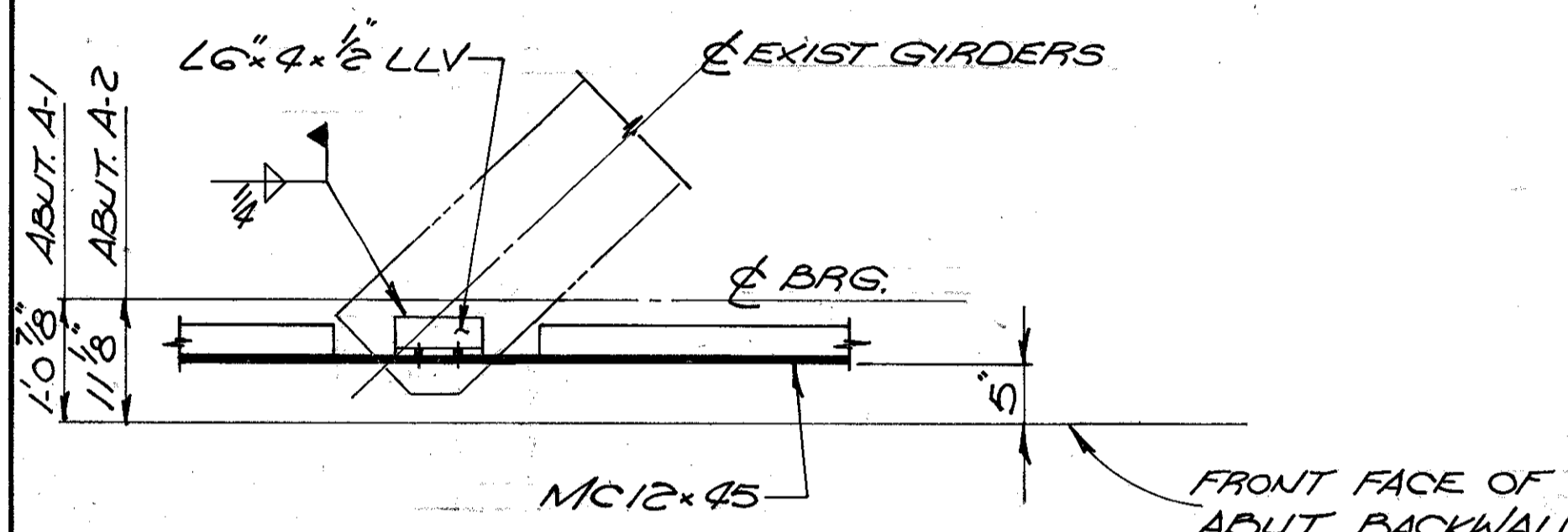


NOTES

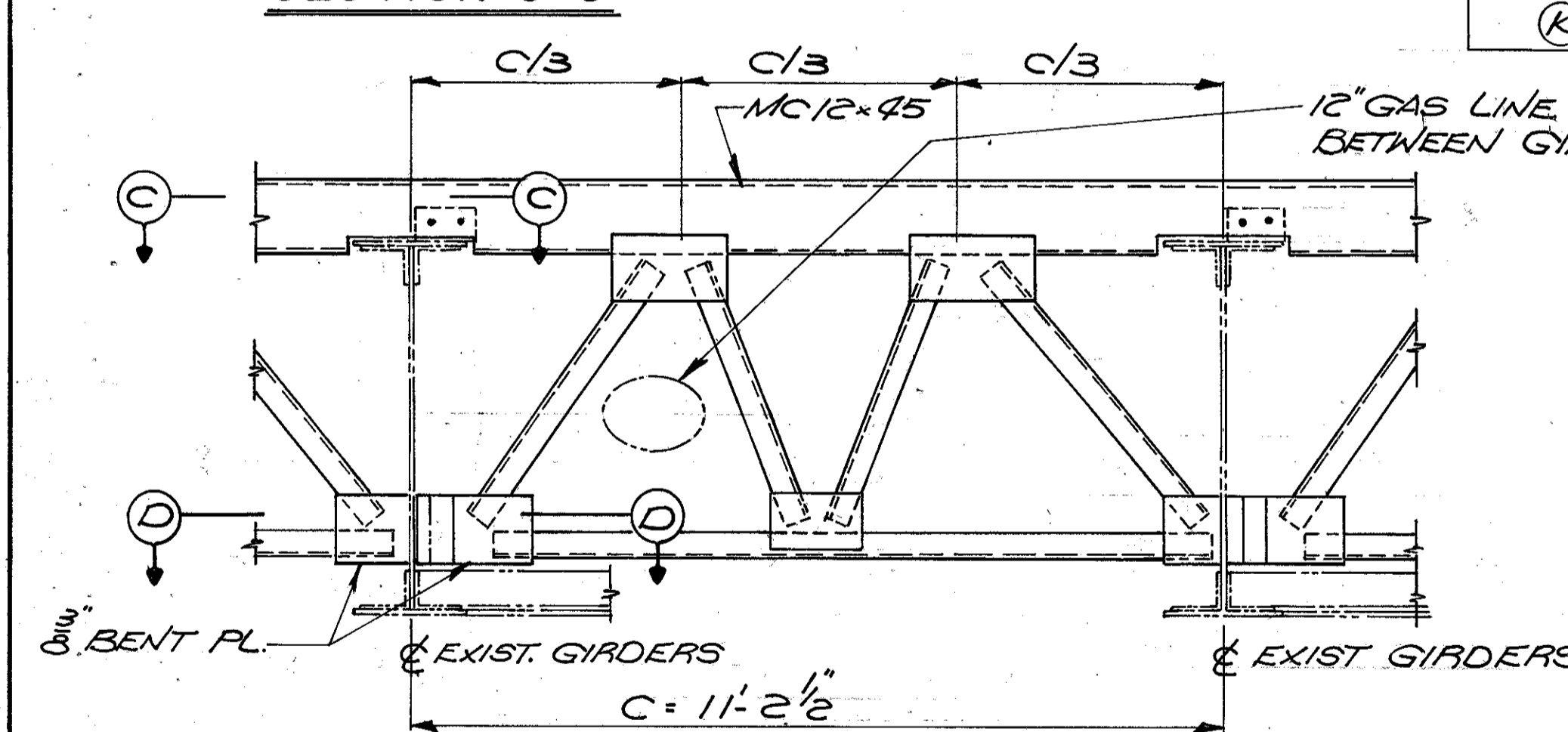
- ALL STRUCTURAL STEEL SHALL BE ASTM A36 UNLESS NOTED OTHERWISE.
- HIGH STRENGTH BOLTS SHALL CONFORM TO ASTM A-325 UNLESS OTHERWISE NOTED.
- FOR ADDITIONAL EXPANSION JOINT AND END CROSSFRAME DETAILS AT ABUTMENTS SEE STANDARD DRAWINGS EXI-4-87 SHEETS 1 THRU 5 AND SD-1-69 SHEET 1 OF 4.
- THE EXPANSION JOINTS AT ABUTMENT A-1 AND A-2 SHALL HAVE A MOVEMENT RATING OF 5". SEE SHEET [2/16] FOR DIMENSION "A" TABLE.
- THE DECK REPLACEMENT WILL BE MADE IN 2 PHASES. CUT EXISTING INTERMEDIATE CROSSFRAMES IN CENTER BAY FROM ABUTMENT A-1 TO PIER P-2 TO FACILITATE JACKING THE GIRDERS AT ABUTMENT A-1 AND PIER P-1. CROSSFRAMES WILL BE RE-ATTACHED AT THE COMPLETION OF PHASE II CONSTRUCTION.
- ABUTMENT A-1 HAS SETTLED APPROXIMATELY 6 INCHES. NEW CONCRETE PEDESTALS WILL BE CONSTRUCTED AT EACH GIRDER SEAT. SEE SHEET [6/16] FOR PEDESTAL DETAIL AND BEAM SEAT ELEVATIONS. ALL BEARINGS AT ABUT. A-1, PIER P-1, & PIER P-2 SHALL BE CLEANED PRIOR TO BEING RESET.
- PIER P-1 HAS SETTLED A MAXIMUM OF 2 3/4". GIRDER SEAT ELEVATIONS SHALL BE ADJUSTED BY USING STEEL PLATE SHIMS.
- IN ORDER TO CONSTRUCT NEW PEDESTALS AT ABUTMENT A-1, JACKS MUST BE PLACED UNDER THE END CROSSFRAMES. THEREFORE DO NOT REMOVE ABUTMENT A-1 CROSSFRAMES UNTIL BEARING ELEVATIONS.
- SEE STRUCTURAL GENERAL NOTES FOR ADDITIONAL NOTES.



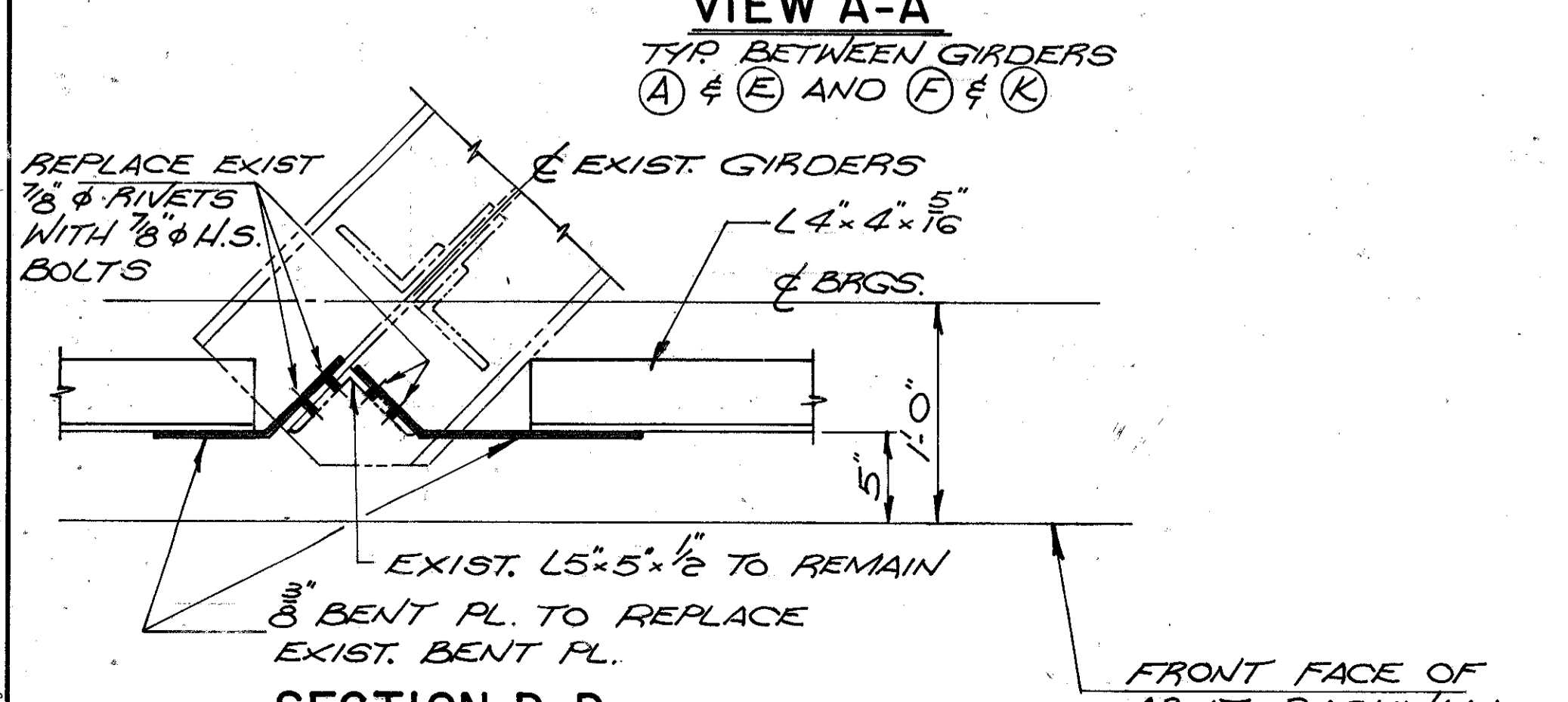
FRAMING PLAN



SECTION C-C

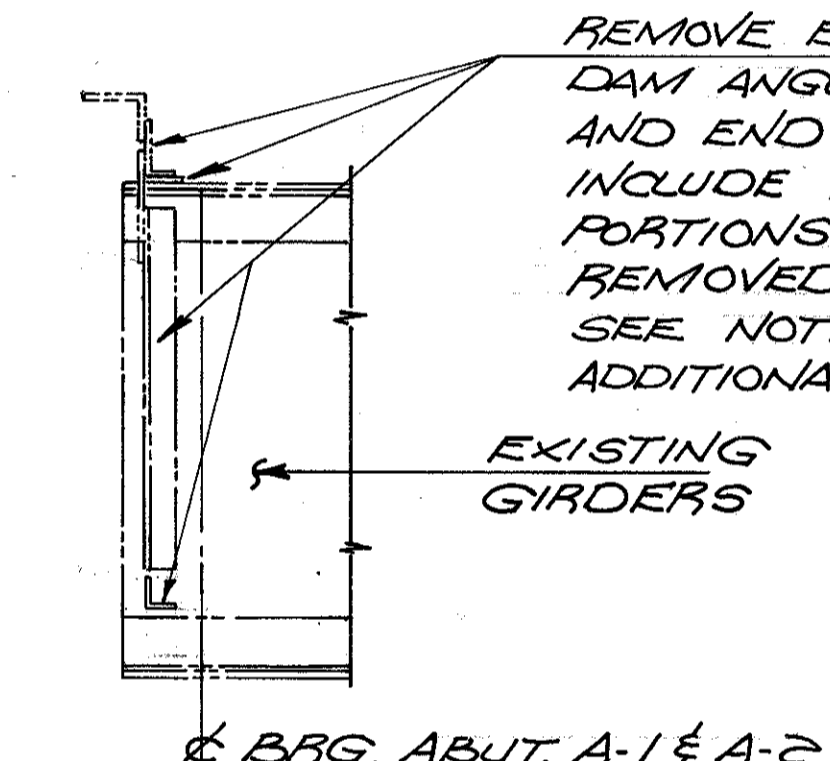


VIEW A-A

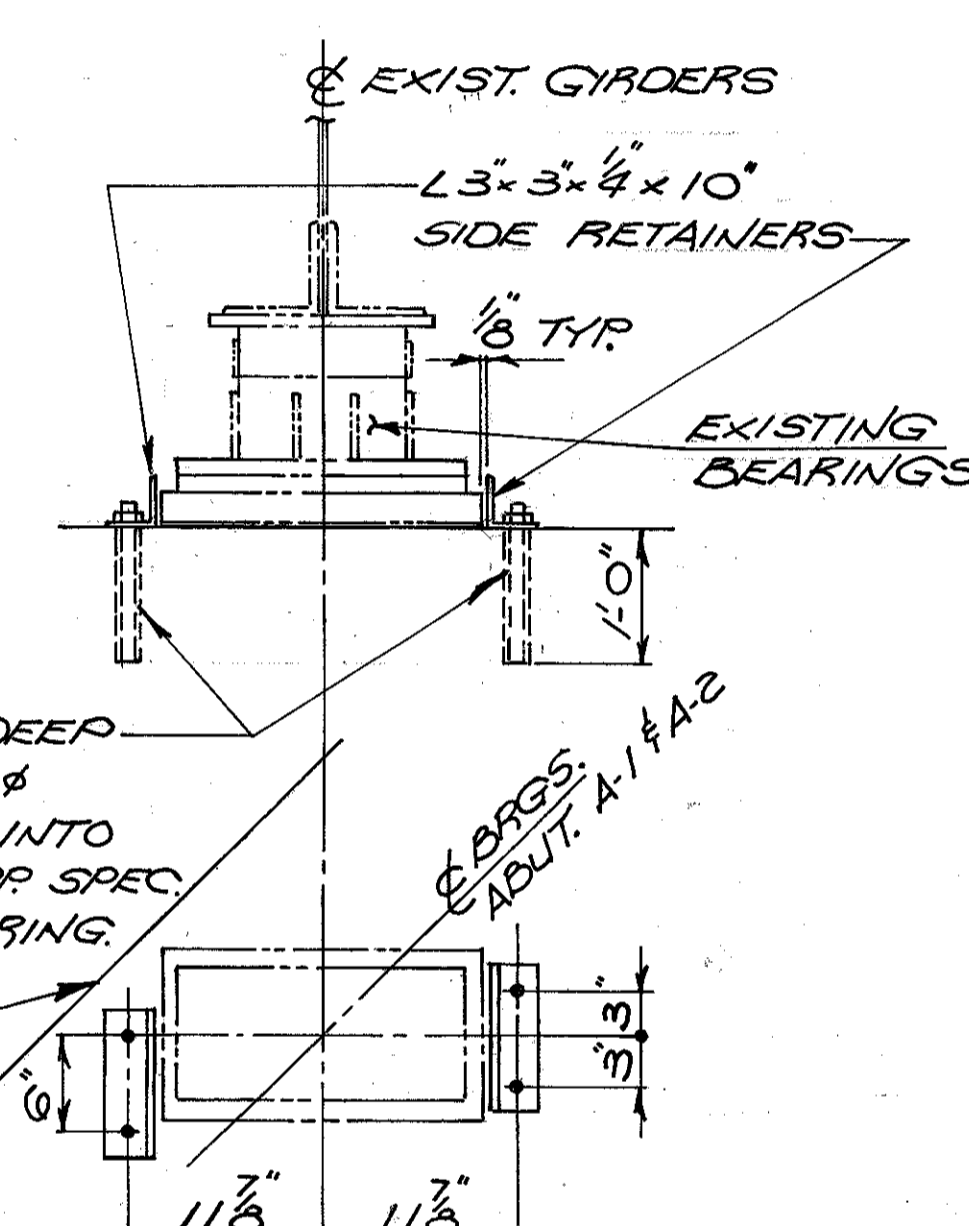
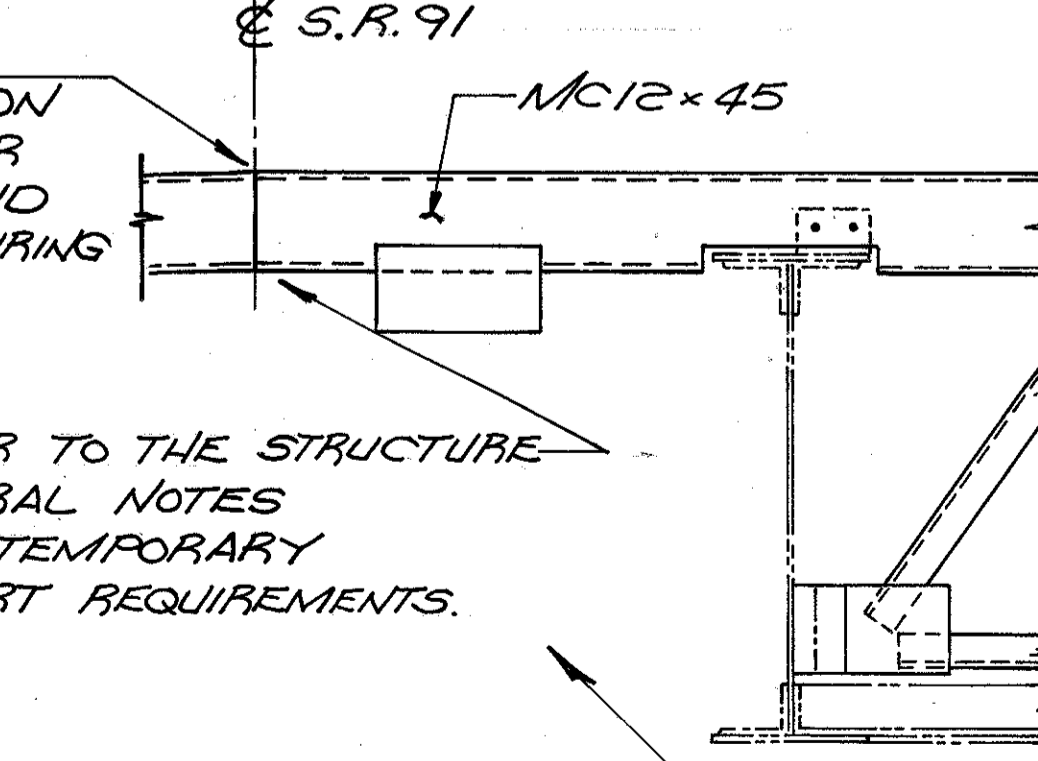


SECTION D-D

SECTION E-E

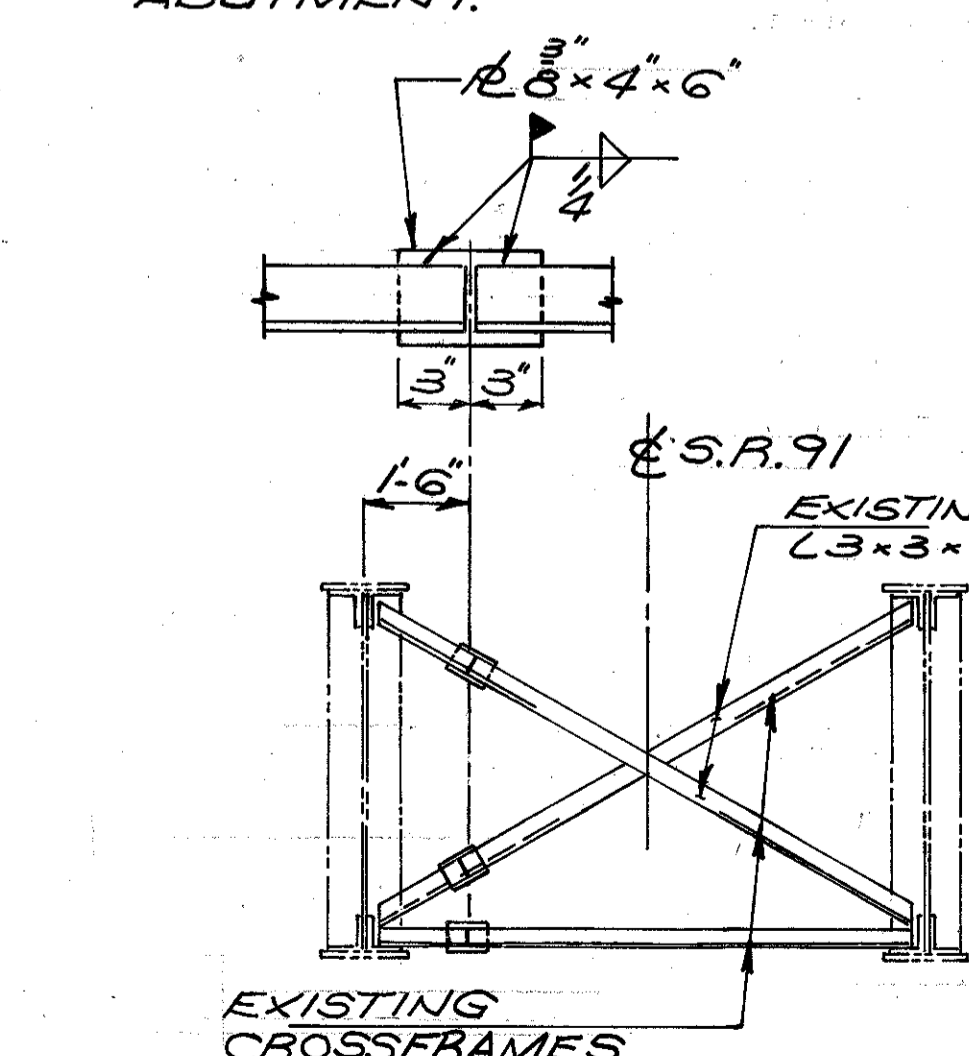


SECTION B-B



BEARING RESET DETAILS

TYPICAL AT EACH ABUTMENT.



SECTION F-F

SEE NOTE (5)

URS			
10/16			
FRAMING DETAILS			
BRIDGE N° LAK-90-0328			
S.R.91 OVER I-90			
LAKE COUNTY		WILLOUGHBY HILLS	
STA 26+74.51 TO STA 33+04.07			
DESIGNED	DRAWN	CHECKED	REVIEWED
Jtl	JWN	Jtl	Jtl
		10-17-90	