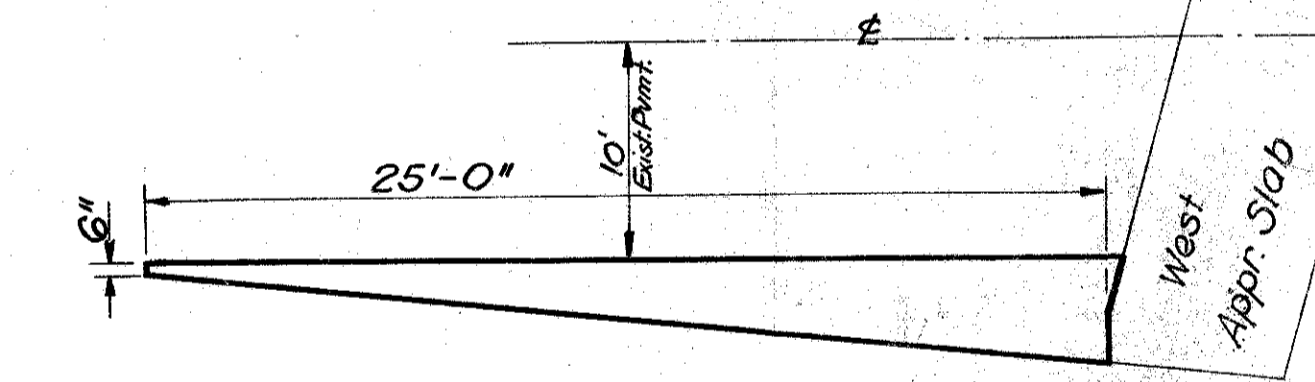
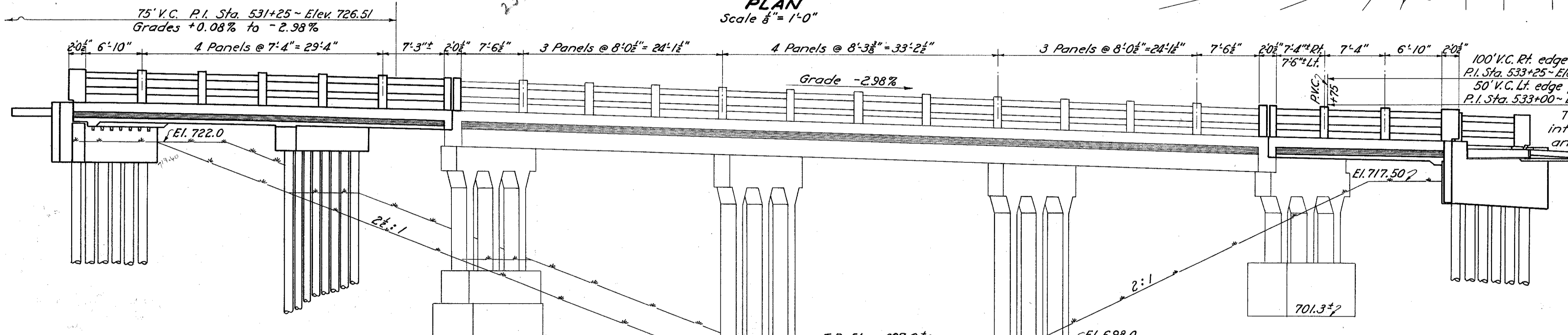


PILING LAYOUT

Piles - 12" Cast-in-place
 Row "A" = 6 @ 36' approx. length
 Row "B" = 8 @ 43' approx. length
 Row "C" = 6 @ 50' approx. length
 Piles shall be driven to a minimum bearing of 26 tons and preferably 30 tons in conformance with supplemental Specification 125 (Revised 5-9-40)



DETAIL OF CONCR. WIDENING @ S.W. APPR

ESTIMATED QUANTITIES

ITEM	ROADWAY & PAVEMENT	QUANTITY	UNIT
I-3	12" Outlet pipe for roadway drainage (including bends)	120	Lin. Ft.
I-8	N° 2-2 C.B.'s	2	Each
E-12	12" pipe removed & stored.	2	Lin. Ft.
E-8	Removal & Disposal of Existing Concrete Pavement & Appr. Slabs	222	Sq. Yds.
T-70	Plain Concrete Pavement (9" thick)	16	Sq. Yds.
E-1	Roadway Excavation	5	Cu. Yds.
S-5	Reinforced Concr. Appr. Slab.	42.7	Sq. Yds.

ELEVATION

Scale 1/8" = 1'-0"

ITEM	BRIDGES	QUANTITY	UNIT	CHG. DRG.	REV.	ASLT.
S-1	Class "C" Concrete, Superstructure (Incl. slab & pier cap)	160	Cu. Yds.	CO3-13	147	COV
S-1	Class "C" Concrete, Abutment Caps.	41	Cu. Yds.			
S-3	Type "B" Waterproofing	22.2	Sq. Yds.			
S-4	Reinforcing Steel	31,070	Lbs.			
S-7	Structural Steel	3,000	Lbs.			
S-9	1" Premoulded Expansion Joint Filler	102	Sq. Ft.			
S-9	Cast Lead Bronze Expansion Plates	344	Lbs.			
S-14	Concrete Railing, including new End Posts on Existing Structure	143.5	Lin. Ft.			
S-14	Repair of Railing on Present Bridge	Lump				
SS-125	First Test Pile (Reinforced Concrete)	Lump				
SS-125	12" Reinforced Concrete Piling	860	Lin. Ft.	CO3-13	782	15L.F.
SS-125	Test Load	Lump				
SS-125	Subsequent Test Load	Lump				
E-2	Excavation for Structures, dry; Abutments	80	Cu. Yds.			
E-2	Excavation for Structures, unclassified (including removal of existing sheet piling and dressing of slopes)	1110	Cu. Yds.	CO3-23	1087	COV
S-29	Copper Tubing for Sidewalk Drains	19	Lin. Ft.			
S-22	Removal of Portions of Existing Structure	18	Cu. Yds.	CO3+2	20	COV
S-23	Dowel Holes for 3/4" Dowel	66	Lin. Ft.			
S-23	Dowel Holes	52	Each			
Spec.	Expansion Bolts (3/4" x 11")	32	Each			

GENERAL NOTES

FOR DETAILS OF APPROACH SLABS NOT SHOWN reference shall be made to Std. Dwg. N° AS-35.

STRUCTURAL STEEL END FINISH AND SIDEWALK DRAIN shall be copper bearing steel. C.O.L. Galv. St. substituted for copper bearing steel.

REMOVAL OF PORTIONS OF EXISTING STRUCTURE- The lines shown on the plans are assumed limits of removal and are subject to change to fit field conditions.

TRAFFIC shall be maintained at all times with watchman service maintained at all times. The structure has been detailed on the assumption that the south half will be constructed complete first, maintaining traffic over the north half, then completing the north half with traffic maintained on the south half. Forms for the slab on the south half shall be left in place until 48 hours after the slab has been placed on the north half.

DISPOSAL OF EXCAVATED MATERIALS- All excess materials excavated under Item E-2 shall be disposed of by the Contractor at his own responsibility and expense outside the limits of the Highway and/or Railroad Right-of-way. Sheet steel piling shall become the property of the Contractor.

CONCRETE RAILING is detailed to match existing plans of the present structure. Care shall be taken to make the new railing as similar to the existing as is practical.

EXCAVATION OR OTHER WORK ON RAILROAD R/W shall be done in co-operation with local officials of the Railroad Company. Any necessary work on the Railroad's telephone line shall be done at the direction of the Engineer in co-operation with local Railroad officials.

REPAIR OF EXISTING RAILING shall be done as directed by the Engineer. Repair work to be done consists of repairing damage caused by lack of provision for contraction and expansion on the present structure. Rail bars have pulled loose from the railing posts at four points. See Sheet #7 for detail of replacement post and expanding joint.

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
BUREAU OF BRIDGES

GENERAL PLAN & ELEVATION
PILING LAYOUT- ESTIMATED QUANTITIES- GENERAL NOTES
BRIDGE N° LA-84-165
OVER B. & O. R.R.

LAKE COUNTY S.H. 563
SEC. D (Pt.) STA. 531+72.56

DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED
E. E. S.	E. E. S.	W. S.	J. H. B.	M. H. R.	10/14/39	6-7-40 9-25-40

Revised As-Drawn 8-4-41 F.C.N.