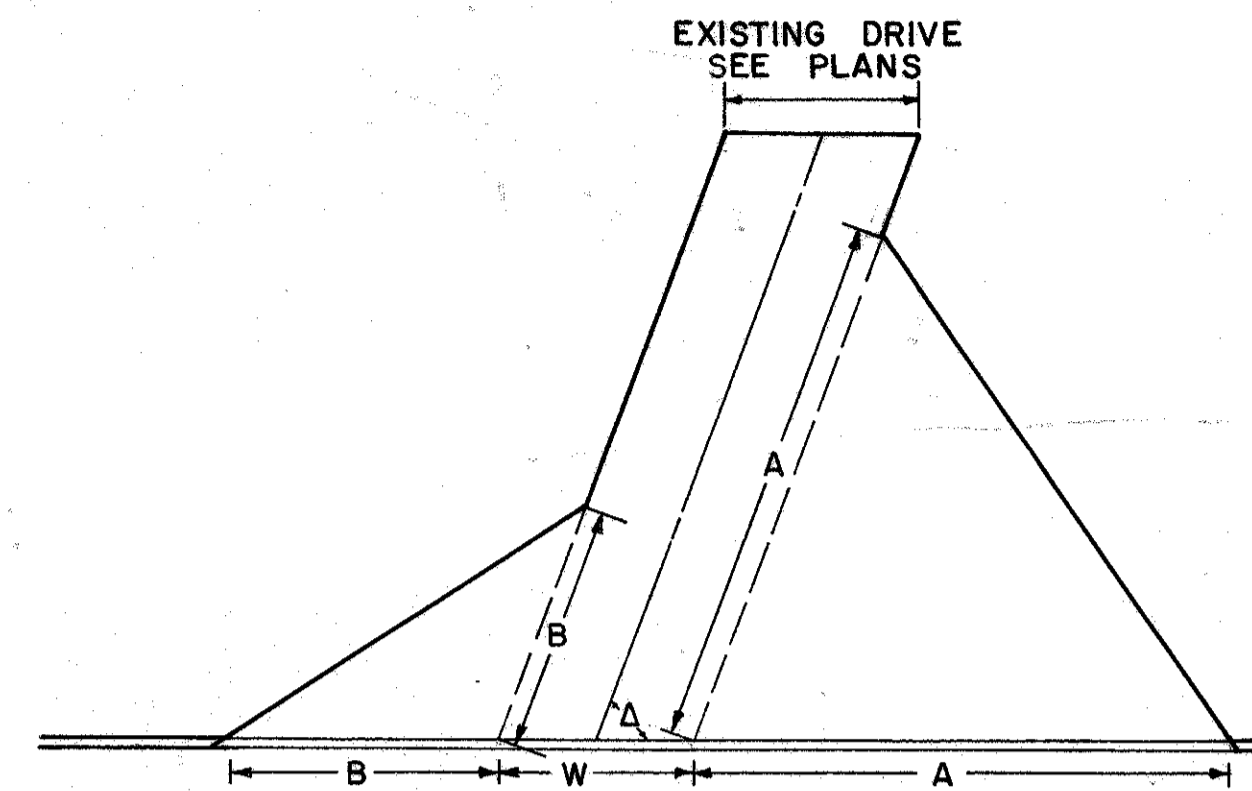
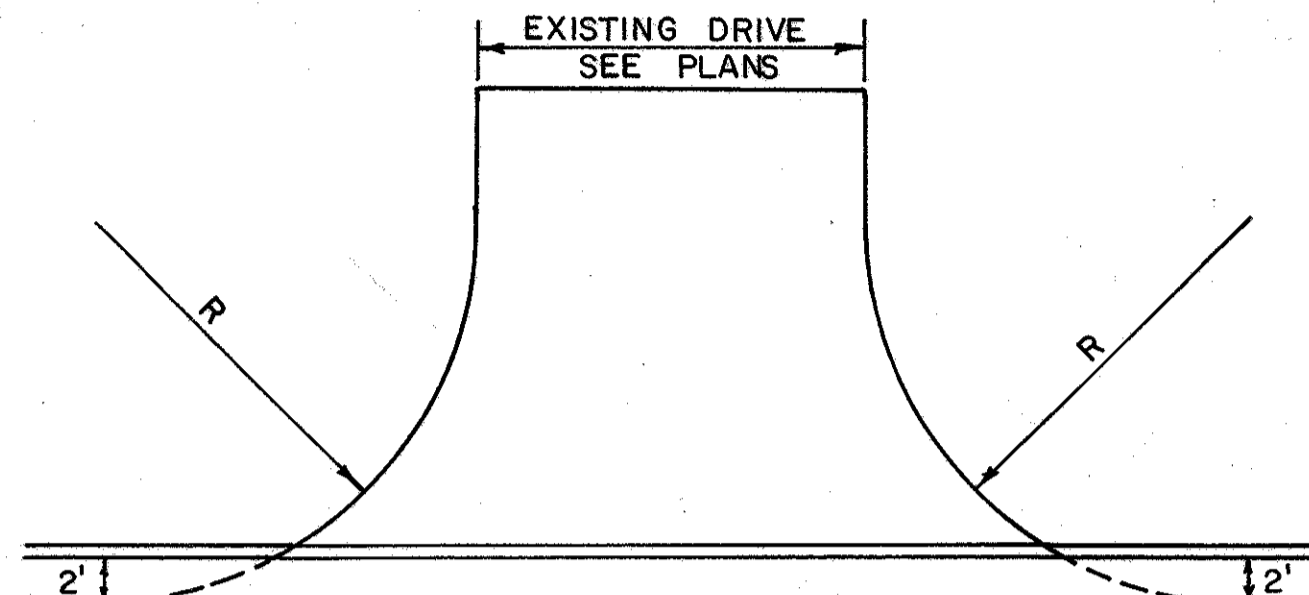


# DRIVEWAY SUMMARY

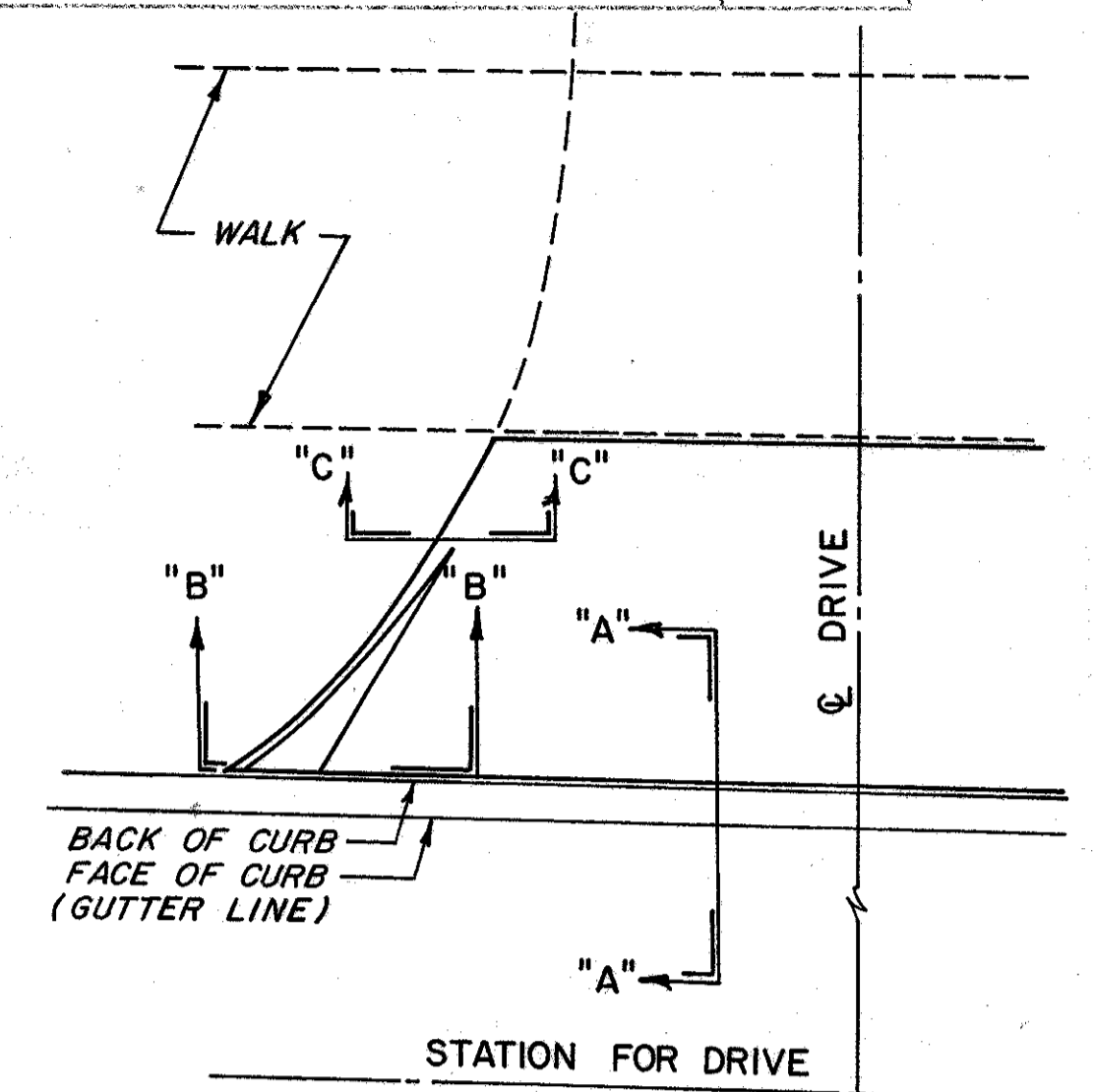
SHEET No.	REFERENCE No.	STATION	SIDE	REPLACEMENT LENGTH			"W" DRIVE WIDTH	TYPICAL DRIVE SECTION	EMBANKMENT	EXCAVATION NOT INCLUDING EMBANKMENT CONSTRUCTION	
				CONC.	ASPH.	STONE				CY.	CY.
133	P-6	41+97	RT.			22.5'	10'	D	9.3	8.1	
133	P-7	44+35	LT.	28.0'			20'	B	0	22.1	
134	P-11	45+65	RT.			24.7'	10'	D	0	11.1	
134	P-12	48+50	RT.			68.5'	10'	D	0	23.1	
134	P-13	49+54.5	RT.	20.5'			34'	A	0	14.4	
135	P-7	50+99	RT.	15.0'	10.5'		20'	A/B	0	10.0	
135	P-8	53+35	RT.	35.5'			25'	A	0	22.7	
135	P-17	55+65	RT.	29.2'			20'	A	0	26.6	
137	P-12	73+90	LT.	31.7'			20'	A	0	23	
138	P-10	76+05	RT.	24.5'		4.0'	11'	A/D	0	8.3	
138	P-11	76+53	RT.	24.5'		11.0'	11'	A/D	0	10.4	
138	P-12	77+08	RT.	24.5'		15.9'	12'	A/D	0	12.9	
138	P-13	77+60	RT.	25.5'		14.5'	11'	A/D	0	11.7	
138	P-14	78+10	RT.	24.6'		19.4'	10'	A/D	0	11.9	
138	P-15	78+64	RT.	24.5'		5.0'	10'	A/D	0	7.9	
138	P-16	79+08	RT.	24.3'	7.7'		10'	A/C	0	8.1	
138	P-17	79+60	RT.	38.0'			11'	A	0	9.9	
138	P-18	80+12	RT.	20.4'		10.1'	11'	A/D	0	8.8	
138	P-19	80+65	RT.	16.5'		12.0'	11'	A/D	0	8.2	
139	P-18	83+96	RT.	33.0'			10'	A	0	8.2	
139	P-19	84+19	RT.	47.5'			10'	A	0	9.0	
* 141	P-10	89+93	LT.	25.0'		39.0'	24'	A/D	42.0	39.0	
* 144	P-6	119+85	RT.	25.0'	40.5'		16'	A/B	0	27.5	
145	P-10	124+27	RT.	30.0'			14'	A	0	21.2	
145	P-11	125+23	RT.	22.0'			12'	A	0	16.3	
BURTON											
139	P-17	0+52	LT.	32.0'			9'	A	0	7.4	
139	P-16	1+05	LT.	28.0'			10'	A	0	6.3	
139	P-15	1+42	RT.	8.0'			9'	A	0	6.1	
HIGH ST. NORTH											
139	P-21	1+34	LT.	20.0'			22'	A	0	14.0	
139	P-20	0+81.5	RT.	30.0'			44'	A	0	35.5	
SECOND ST.											
147	P-9	1+74	LT.	25.0'			9'	A	0	6.3	
147	P-10	1+80	RT.	20.0'		5.0'	10'	A/D	0	6.8	
147	P-11	2+41	RT.	20.0'		5.0'	10'	A/D	0	6.6	
* 144	P-7	116+10	RT.	48.0'	136.0'		20'	A/B	34.0	120.5	



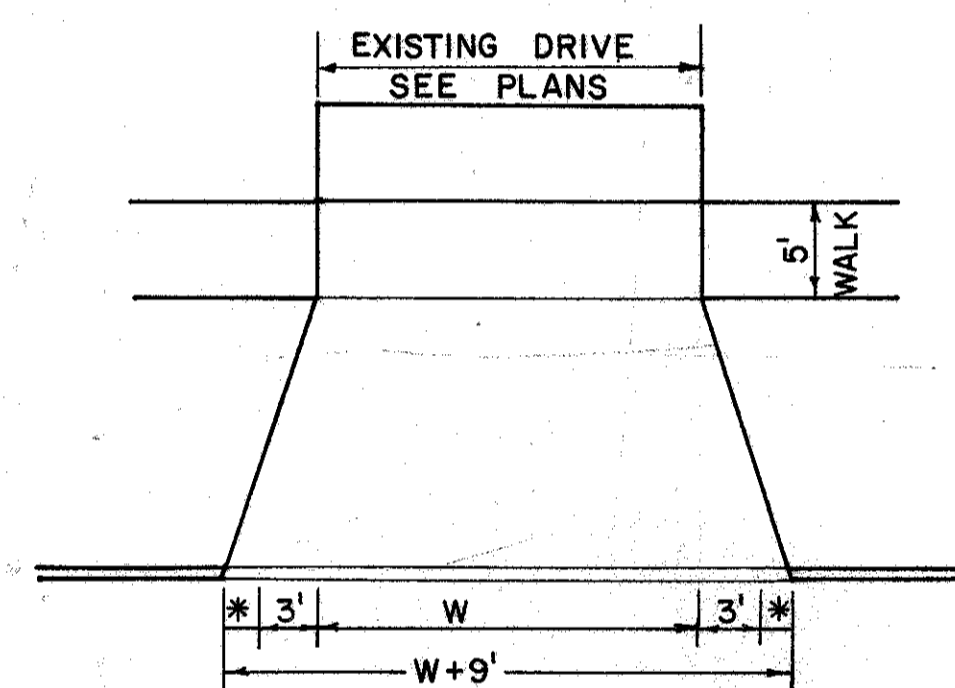
TYPICAL DRIVE DETAIL



TYPICAL DRIVE DETAIL

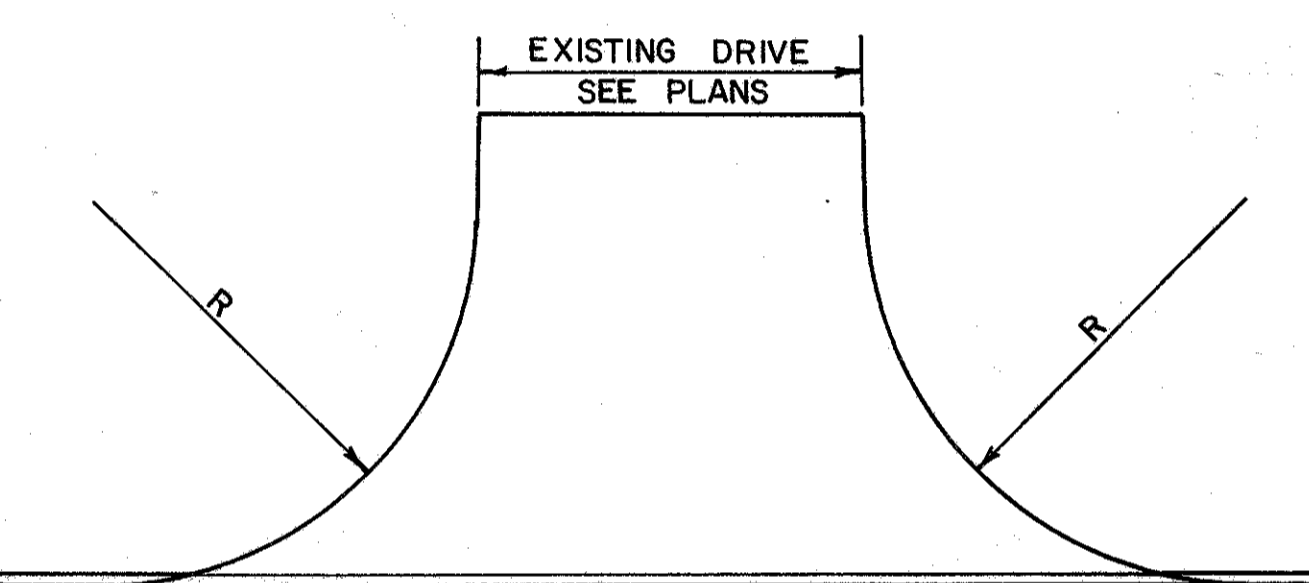


DRIVE APRON CURB PLAN

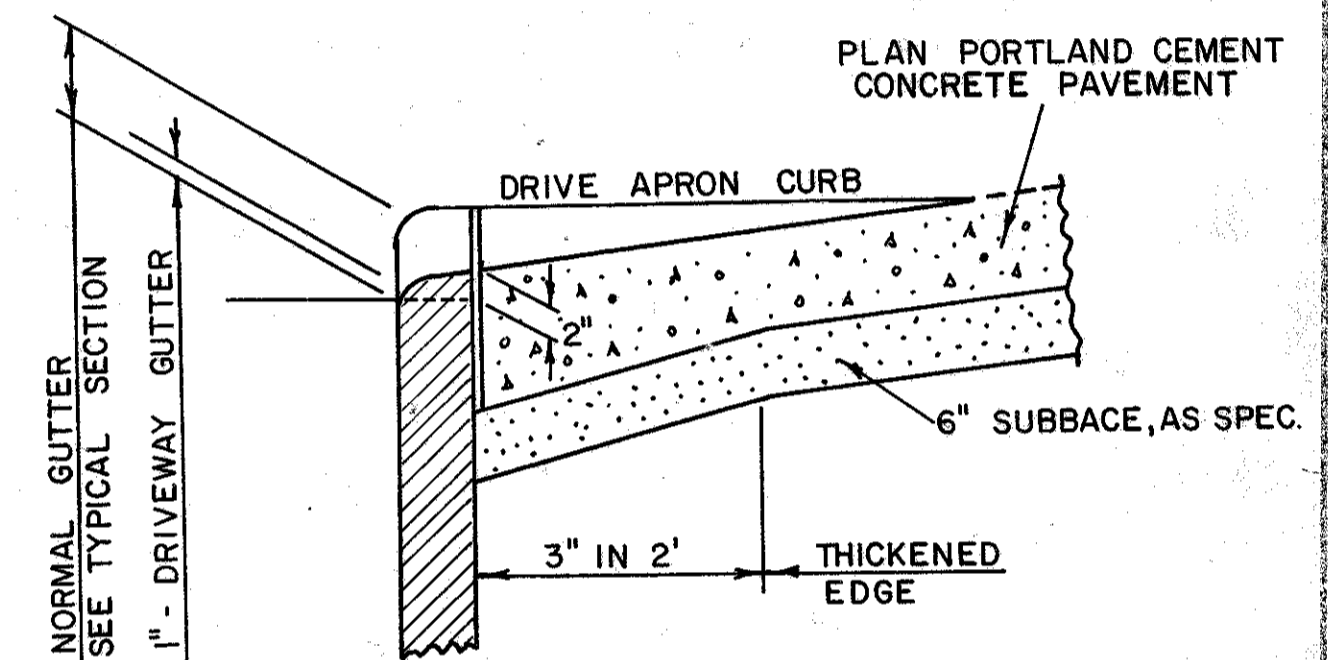


TYPICAL DRIVE DETAIL

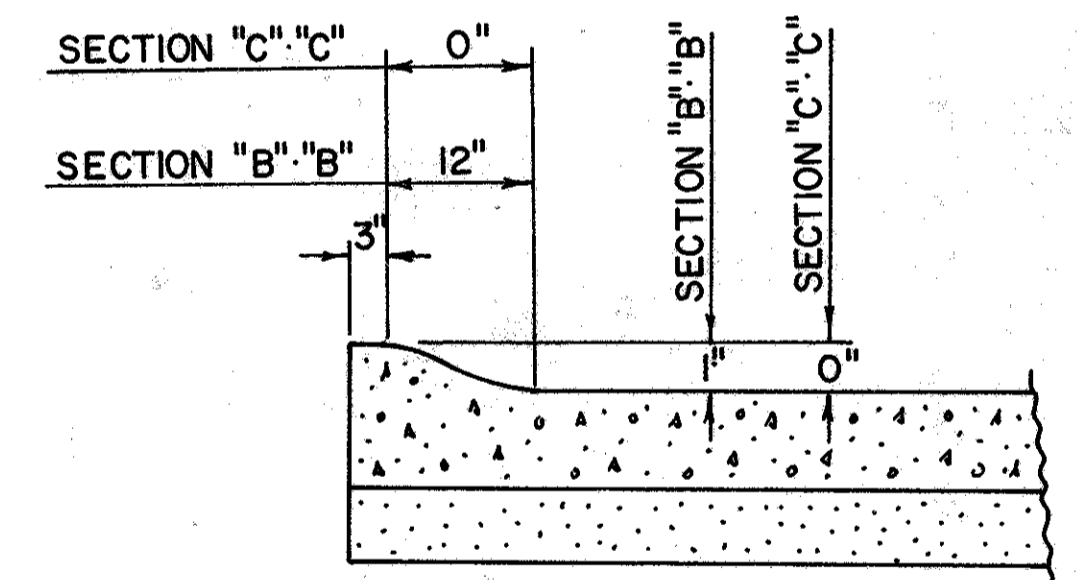
\* 1'-6" CURB DROP



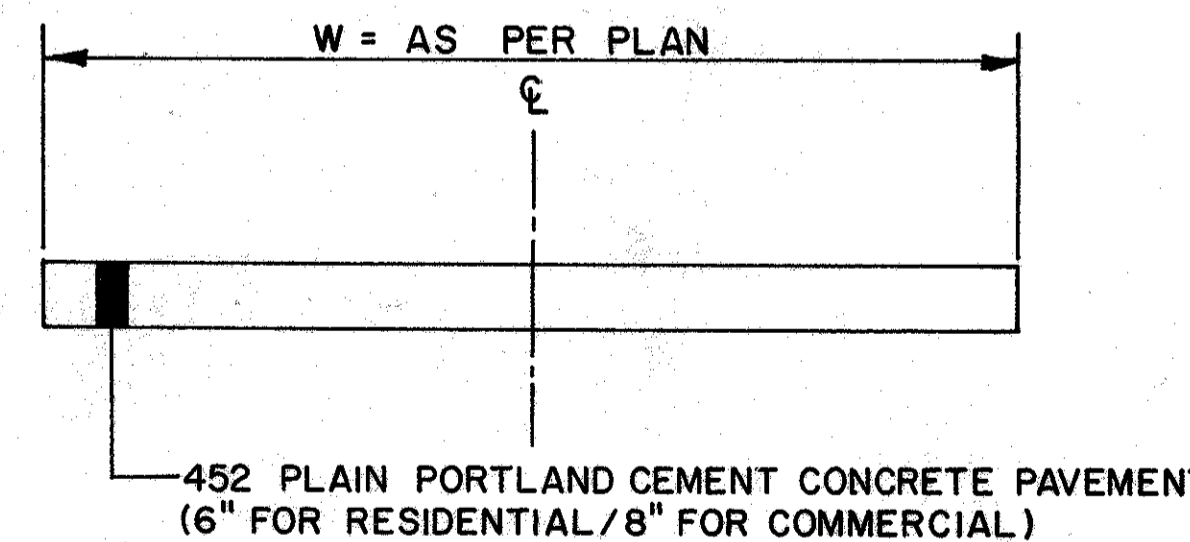
TYPICAL DRIVE DETAIL



SECTION "A"- "A"  
(WITH BARRIER CURB)

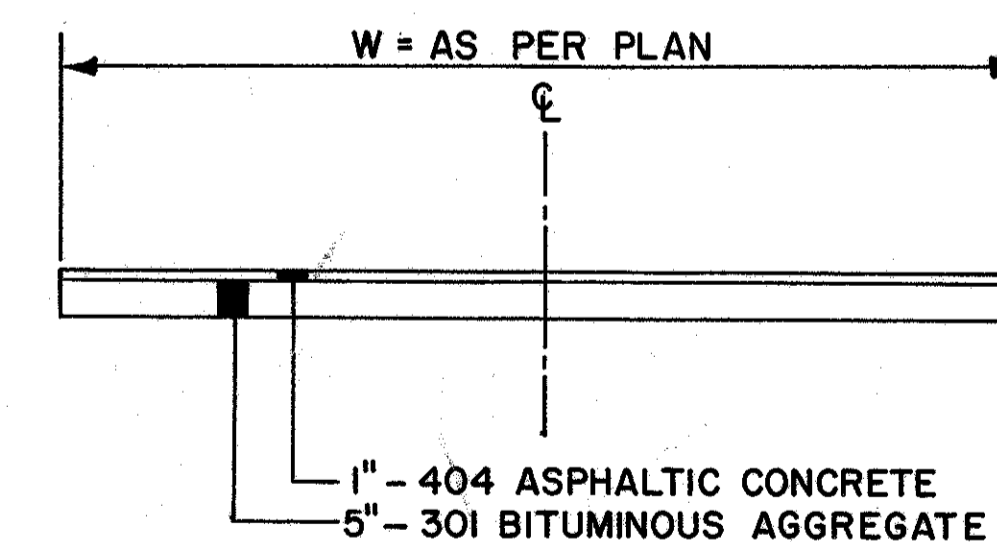


SECTION "B"- "B" & "C"- "C"  
(DETAIL APRON CURB)



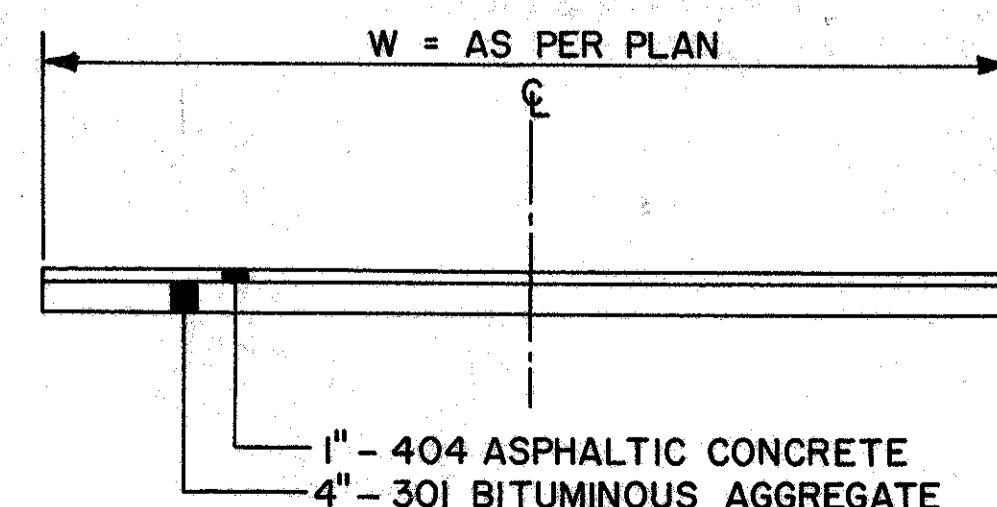
TYPICAL DRIVE SECTION A

452 PLAIN PORTLAND CEMENT CONCRETE PAVEMENT  
(6" FOR RESIDENTIAL/8" FOR COMMERCIAL)



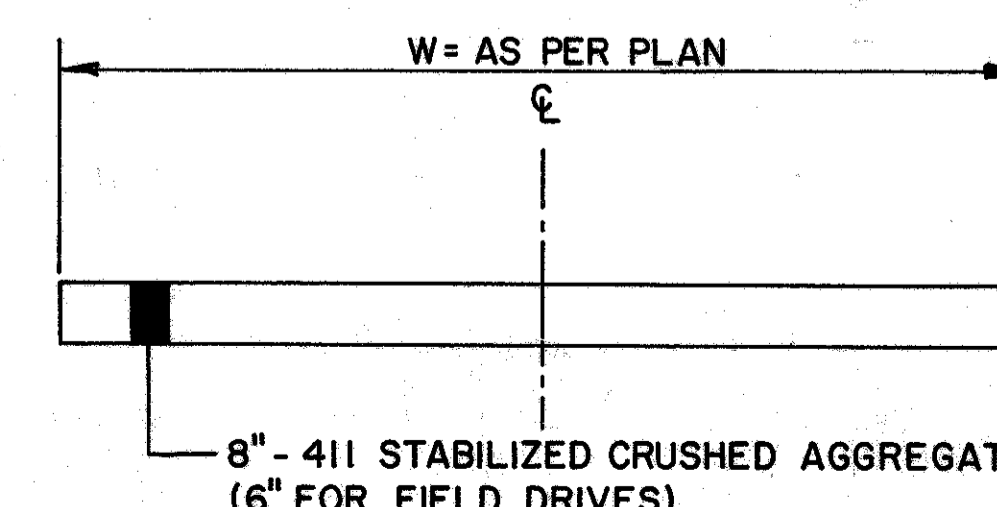
TYPICAL DRIVE SECTION B

1" - 404 ASPHALTIC CONCRETE  
5" - 301 BITUMINOUS AGGREGATE



TYPICAL DRIVE SECTION C

1" - 404 ASPHALTIC CONCRETE  
4" - 301 BITUMINOUS AGGREGATE



TYPICAL DRIVE SECTION D

8" - 411 STABILIZED CRUSHED AGGREGATE  
(6" FOR FIELD DRIVES)

**NOTES:**  
 DRIVE APRON CURB SECTION FOR CONCRETE DRIVES WITH BARRIER CURB IS TO BE PLACED AS DIRECTED BY THE ENGINEER.  
 FORM DRIVE APRON CURB (SECTION "B"- "B") BEHIND CURB LINE AND TAPER OUT UNIFORMLY TO NO CURB (SECTION "C"- "C").  
 THE COST OF ALL LABOR AND MATERIAL NECESSARY TO CONSTRUCT CURB SECTION AND THICKENED EDGE AS SHOWN, SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE BID PER SQ. YD. FOR ITEM 452 PLAIN PORTLAND CEMENT CONCRETE FOR DRIVES.  
 CURB SHALL BE DROPPED TO PROVIDE A 1" GUTTER AT ALL DRIVEWAYS AND WHEREVER DIRECTED BY THE ENGINEER.  
 A SATISFACTORY TRANSITION FROM THE NORMAL CURB HEIGHT TO A 1" HEIGHT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE BID FOR THE PERTINENT CURB SPECIFIED.  
 WHERE DRIVE ABUTS NEW OR EXISTING CONCRETE SIDEWALKS AND NEW OR EXISTING CONCRETE CURBS, 1/2" EXPANSION JOINT MATERIAL 705.03 SHALL BE PROVIDED.  
 CONCRETE DRIVES AND APRONS TO HAVE IMPRESSED JOINT ALONG C OF DRIVE. ADDITIONAL IMPRESSED JOINTS SHALL BE SPACED AT 8FT. CENTERS OR AS DIRECTED BY THE ENGINEER.  
 THE EXPANSION JOINT BETWEEN THE BACK OF CURB AND DRIVE APRON SHALL BE SEALED WITH 705.01, 705.02 OR 705.11 JOINT SEALER.