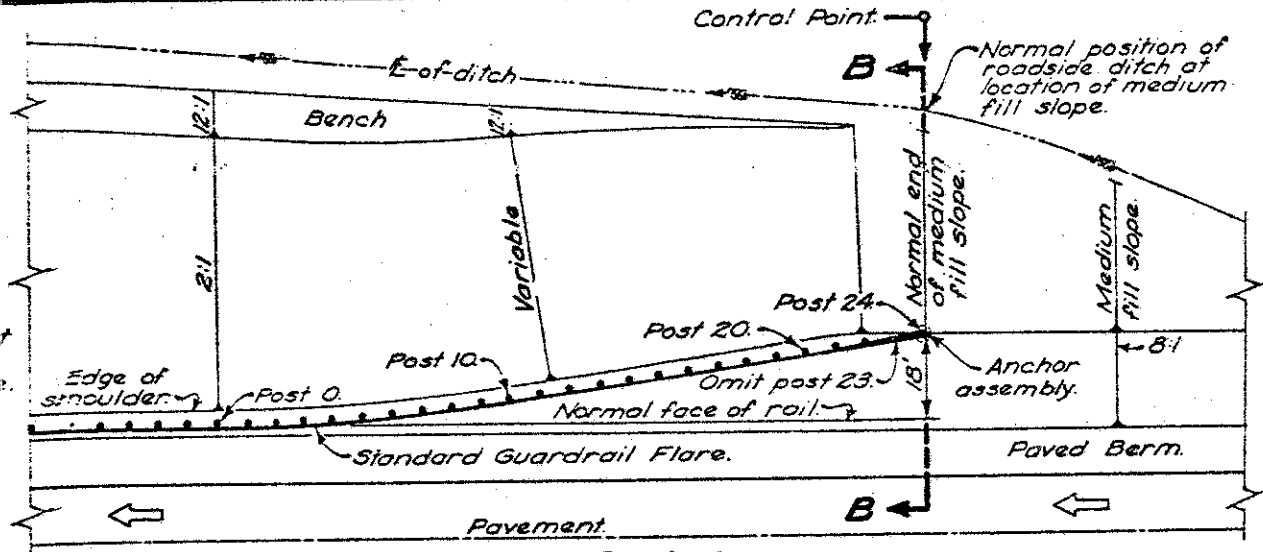
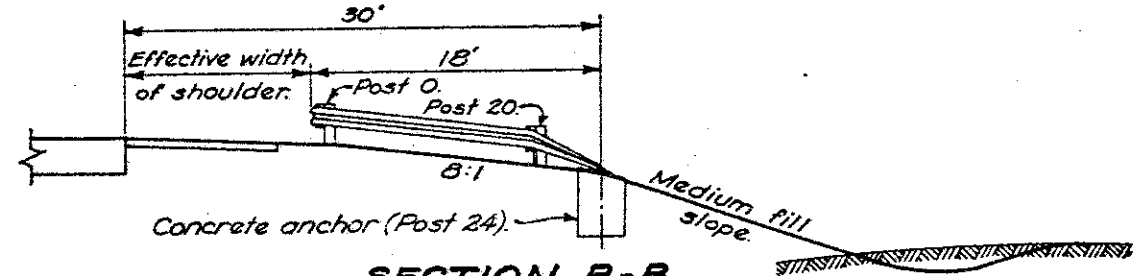


Description	Post No (6'-5" %)	Offset	*Length
Beginning of flare.	0	0.00'	
	1	.05'	
	2	.19'	
	3	.43'	
	4	.76'	
End circular segment (Introduced)	5	1.18'	
	6	1.71'	
	7	2.32'	
	8	3.04'	
	9	3.84'	
End circular segment (F to F (C.A.F.))	10	4.74'	
Terminal-Cut to Cut protection only	20	8.30'	125'
Terminal-Standard roadside flares	24	18.00'	150'
Terminal-Cut to Fill protection only	32	25.58'	200'

* Measured to post bolt hole in special end shoe.



PLAN



SECTION B-B
FILL TO FILL

STANDARD GUARDRAIL FLARE

Post arrangements shown are designed for application to Type 5 installations. For Type 7 guardrail omit odd numbered posts except in end assembly.

NOTES

Design details shown shall govern the construction of guardrail at hazardous cut to fill, fill to fill and in cut areas unless otherwise shown on the plans.

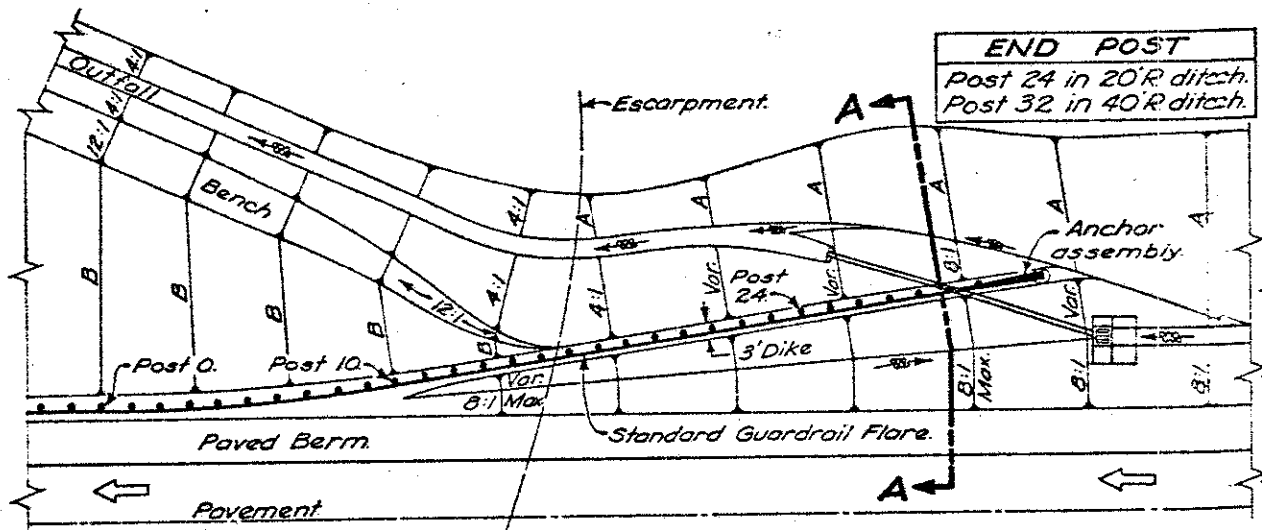
Backslope 'A' and fill slope 'B' shall be constructed as shown on typical sections.

Storm Sewer and Catch Basin shall be constructed only at locations shown on the plan and profile sheets.

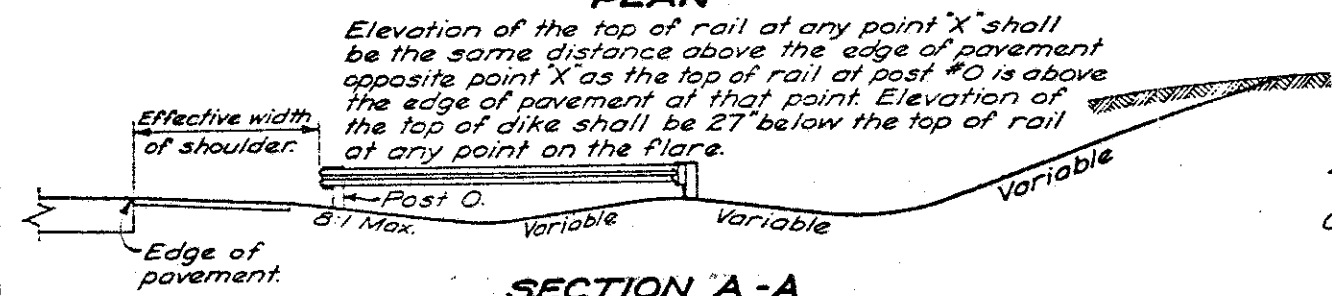
Design shown for guardrail introduced because of an obstruction is applicable to directional roadways. On two-way roadways provide a 125 foot long flare and anchor assembly on both sides of the obstruction.

Type 5 guardrail shall be used for all introduced installations.

For anchor assembly detail see Standard Drawing GR-4.

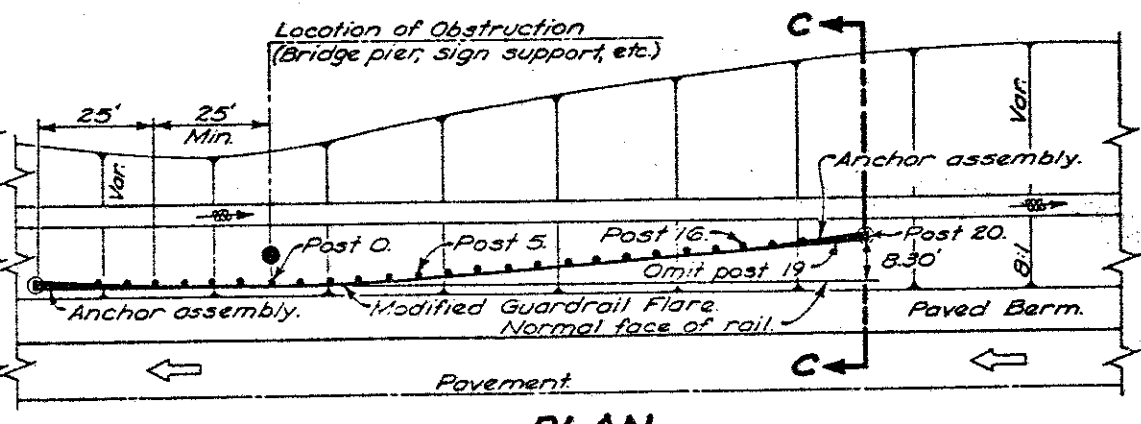


PLAN

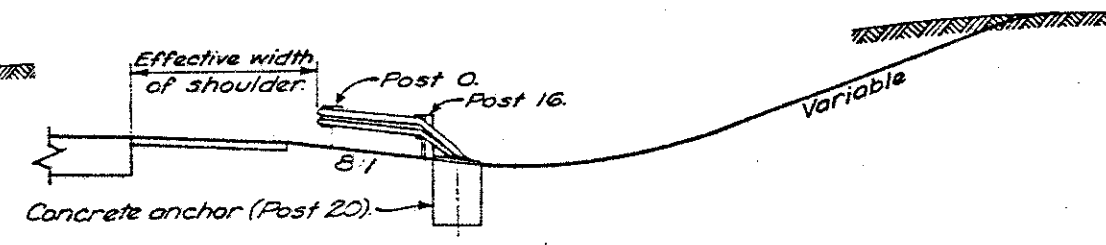


SECTION A-A
CUT TO FILL

Elevation of the top of rail at any point 'X' shall be the same distance above the edge of pavement opposite point 'X' as the top of rail at post #0 is above the edge of pavement at that point. Elevation of the top of dike shall be 27" below the top of rail at any point on the flare.



PLAN



SECTION C-C
INTRODUCED
because of obstruction.

BUREAU OF LOCATION AND DESIGN
OHIO DEPARTMENT OF HIGHWAYS

GUARDRAIL FLARES

DATE 1-15-68
1-1-71

STANDARD CONSTRUCTION DRAWING GR-5

APPROVED *R. L. Little* ENGR. L. & O.