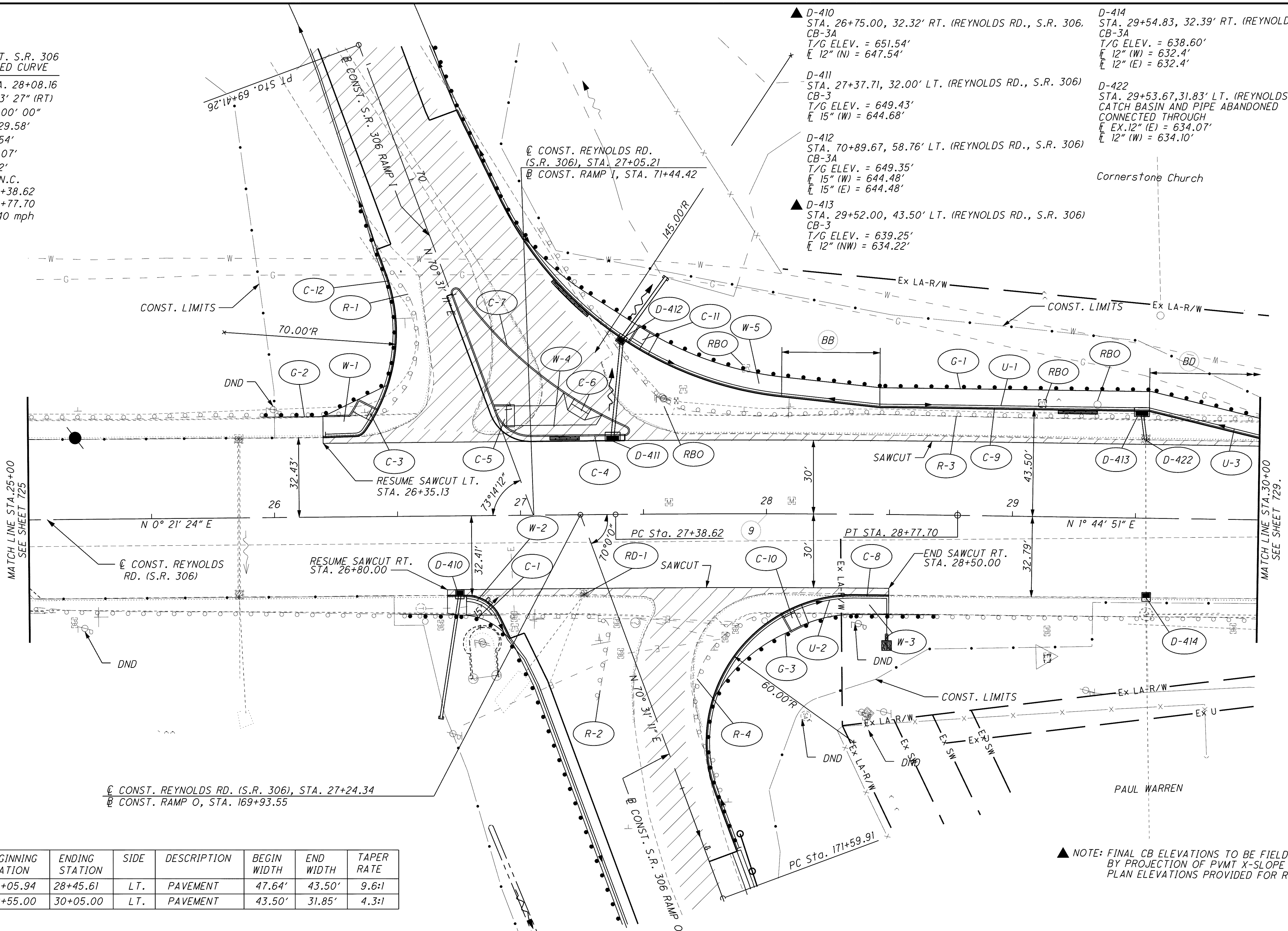


9
 C CONST. S.R. 306
 PROPOSED CURVE
 P.I. STA. 28+08.16
 $\Delta = 1^\circ 23' 27''$ (RT)
 $D_c = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 69.54'$
 $L = 139.07'$
 $E = 0.42'$
 $\epsilon_{max} = N.C.$
 $PC = 27+38.62$
 $PT = 28+77.70$
 $D.S. = 40$ mph



D-410
 STA. 26+75.00, 32.32' RT. (REYNOLDS RD., S.R. 306)
 CB-3A
 T/G ELEV. = 651.54'
 $\epsilon 12''$ (N) = 647.54'

 D-411
 STA. 27+37.71, 32.00' LT. (REYNOLDS RD., S.R. 306)
 CB-3
 T/G ELEV. = 649.43'
 $\epsilon 15''$ (W) = 644.68'

 D-412
 STA. 70+89.67, 58.76' LT. (REYNOLDS RD., S.R. 306)
 CB-3A
 T/G ELEV. = 649.35'
 $\epsilon 15''$ (W) = 644.48"
 $\epsilon 15''$ (E) = 644.48"

 D-413
 STA. 29+52.00, 43.50' LT. (REYNOLDS RD., S.R. 306)
 CB-3
 T/G ELEV. = 639.25'
 $\epsilon 12''$ (NW) = 634.22'

 D-414
 STA. 29+54.83, 32.39' RT. (REYNOLDS RD., S.R. 306)
 CB-3A
 T/G ELEV. = 638.60'
 $\epsilon 12''$ (W) = 632.4'
 $\epsilon 12''$ (E) = 632.4'

 D-422
 STA. 29+53.67, 31.83' LT. (REYNOLDS RD., S.R. 306)
 CATCH BASIN AND PIPE ABANDONED
 CONNECTED THROUGH
 $\epsilon EX. 12''$ (E) = 634.07'
 $\epsilon 12''$ (W) = 634.10'

CALCULATED BSH
 CHECKED MBM
 HORIZONTAL SCALE IN FEET
 0 10 20 30

PLAN - REYNOLDS RD. (S.R. 306)
 STA. 25+00.00 TO STA. 30+00.00

LAK-2-3.32

TAPER CODE	BEGINNING STATION	ENDING STATION	SIDE	DESCRIPTION	BEGIN WIDTH	END WIDTH	TAPER RATE
BB	28+05.94	28+45.61	LT.	PAVEMENT	47.64'	43.50'	9.6:1
BD	29+55.00	30+05.00	LT.	PAVEMENT	43.50'	31.85'	4.3:1

FOR RAMP I PLANS, SEE SHEET 561 - 562
 FOR RAMP O PLANS, SEE SHEET 612 - 613
 FOR PIPE PROFILES, SEE SHEET 580 - 581
 FOR INTERSECTION DETAILS, SEE SHEET 590 - 591
 FOR REYNOLDS ROAD PROFILE, SEE SHEET 728
 FOR UNDERDRAIN DETAILS, SEE SHEET 950

DND = DO NOT DISTURB
 ITEM 202 - ITEM REMOVED
 RBO RELOCATED/REMOVED BY OTHERS

▲ NOTE: FINAL CB ELEVATIONS TO BE FIELD DETERMINED BY PROJECTION OF PVMT X-SLOPE FROM SAWCUT. PLAN ELEVATIONS PROVIDED FOR REFERENCE ONLY.