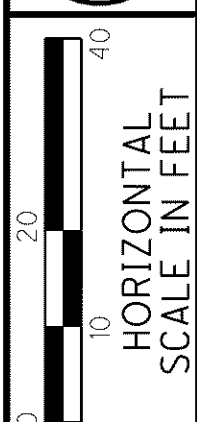


FOR MAINTENANCE OF TRAFFIC PHASING
PLANS FOR STAGE 1 SEE SHEETS 109 TO 130

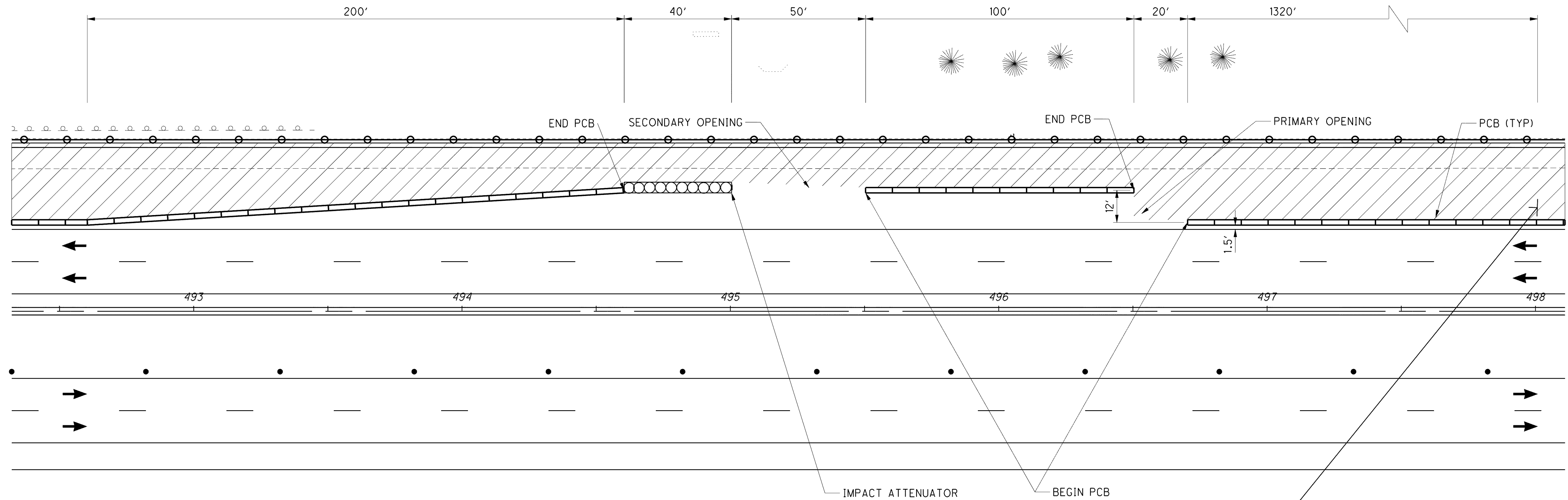
FOR MAINTENANCE OF TRAFFIC PHASING
PLANS FOR STAGE 2 SEE SHEETS 131 TO 145

FOR MAINTENANCE OF TRAFFIC PHASING
PLANS FOR STAGE 3 SEE SHEETS 146 TO 159



CALCULATED JMM
CHECKED DRJ

NOTE: IMPACT ATTENUATORS OMITTED IF
SECONDARY OPENING IS NOT PROVIDED



CONSTRUCTION ACCESS POINTS TYPICAL

CONSTRUCTION ACCESS POINT LOCATIONS MAY BE SELECTED BY THE CONTRACTOR, WITH THE APPROVAL OF THE ENGINEER. THEY SHALL BE IN ACCORDANCE WITH THE DETAILS IN THE PLANS. THE LOCATION SHALL BE SELECTED FOR GOOD SIGHT DISTANCE AND EASE OF ACCESS FOR ENTERING VEHICLES (AVOID LOCATIONS JUST BEYOND CREST VERTICAL CURVES, ON CURVES TO THE SIDE OF THE ROAD WITH THE BARRIER, JUST BEYOND OVERHEAD STRUCTURES, ON UPGRADES, ETC.) IN THE EVENT THAT THE ENGINEER DETERMINES THAT AN ACCESS POINT DOES NOT FUNCTION IN A SAFE MANNER, HE SHALL ORDER IT IMMEDIATELY CLOSED AT NO COST TO THE STATE. ACCESS POINTS WHICH INCLUDE THE SECONDARY OPENING SHALL ALSO INCLUDE AN OPERABLE IMPACT ATTENUATOR AS DETAILED. WHEN AN IMPACT ATTENUATOR BECOMES INOPERABLE FOR ANY REASON, THE ENGINEER SHALL ORDER THE SECONDARY OPENING TO BE IMMEDIATELY CLOSED WITH PORTABLE CONCRETE BARRIER. ACCESS POINTS MAY BE RELOCATED, SUBJECT TO THE APPROVAL OF THE ENGINEER, AS NECESSARY TO ACCOMPLISH CONSTRUCTION ACTIVITIES.

ALL COSTS FOR RELOCATION OF PORTABLE CONCRETE BARRIER, INSTALLATION, REPAIR, REPLACEMENT, AND REMOVAL OF IMPACT ATTENUATORS, GRADING FOR ACCESS DRIVES AND RELATED COSTS SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC.

MAINTENANCE OF TRAFFIC
CONSTRUCTION ACCESS POINTS, TYPICAL

LAK - 2 - 3.32

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1679

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