

MICROFILMED

JUL 8 1986

FED. RD. DIVISION	STATE	PROJECT	299 384
2	OHIO		

LAKE COUNTY
SEC. LAK-2-14.22

GENERAL NOTES

REFERENCE shall be made to Standard Drawings C.S.B.-2-56, Sheets 2 and 3 of 6 revised 2-2-59, B-1-55 revised 2-2-59, AR-1-57 revised 2-2-59, AS-1-54 revised 12-1-54, Suppl. Spec's. S-307 dated 8-23-60 and M-206.14 dated 7-15-49.

DESIGN SPECIFICATIONS: This structure conforms to the requirements of "Design Specifications for Highway Structures" of the State of Ohio, Department of Highways, dated 9-1-57 together with current revisions thereof.

CONSTRUCTION CLEARANCE of 19' vertically above the top of the railroad rails and 8' horizontally from the center of tracks shall be maintained at all times.

RAILROAD AERIAL LINES shall be relocated by the railroad. The CONTRACTOR shall use all precautions necessary to see that the lines are not disturbed during the construction stage and shall cooperate with the railroad in the relocation of these lines. The cost of the relocation shall be included in the railroad force account work.

UTILITY LINES: All labor and expense involved in relocating (installing) the affected utility lines, not included in this contract, shall be borne by the owners. The contractor and owners are requested to cooperate by arranging their work in such a manner that inconvenience to either will be held to a minimum.

SHEETING AND BRACING: Before construction is started, eight sets of prints showing details of the sheeting and bracing to be used for excavation adjacent to the railroad tracks shall be submitted to the Director for approval by the Department of Highways and by the Railroad Company.

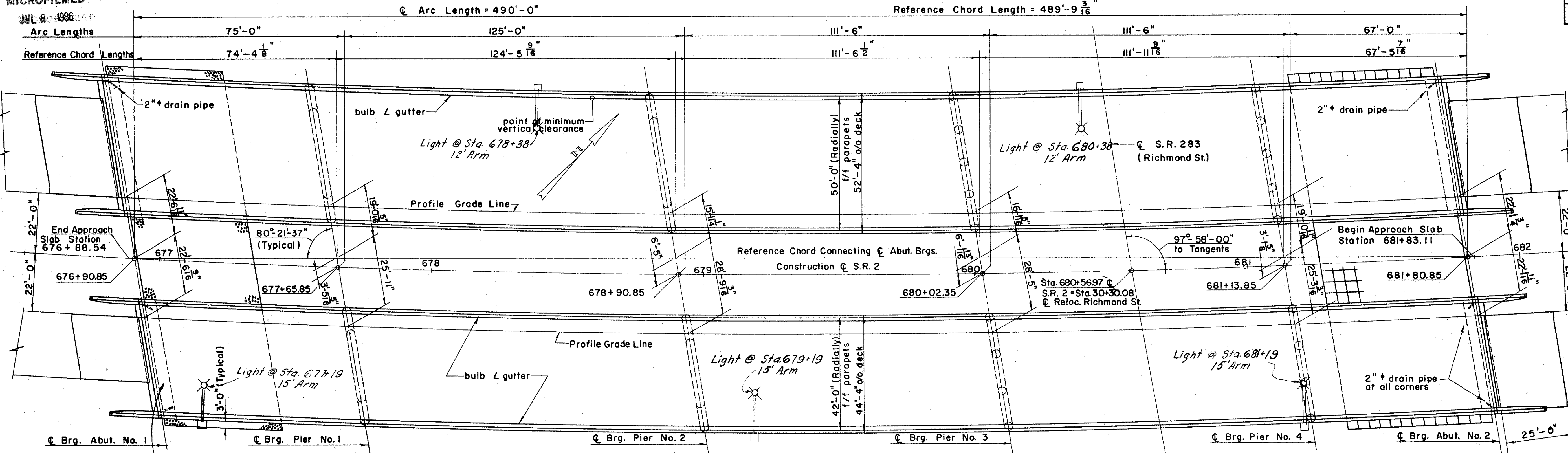
ALIGNING RAILROAD TRACKS: After the contractor has completed all excavation and backfill adjacent to the railroad tracks in compliance with Sec. E-2.04 and E-2.08 of the Construction and Material Specifications, subject to the supervision of the Railroad Company, nothing in Sec. E-2.04, E-2.08 or G-8.07 of the Specifications shall be construed to hold the contractor liable for aligning and re-surfacing the railroad tracks.

EXCAVATION: Excavation quantity includes the removal of fill material between the surface of proposed embankment and the bottom of the footings.

PILES shall be driven to a minimum bearing capacity of 40 tons per pile for the abutments and 35 tons per pile for the piers.

POROUS BACKFILL shall extend the full length of the abutment backwall and upward to the approach slab. Excavation for same is included in the bid price per cubic yard of porous backfill.

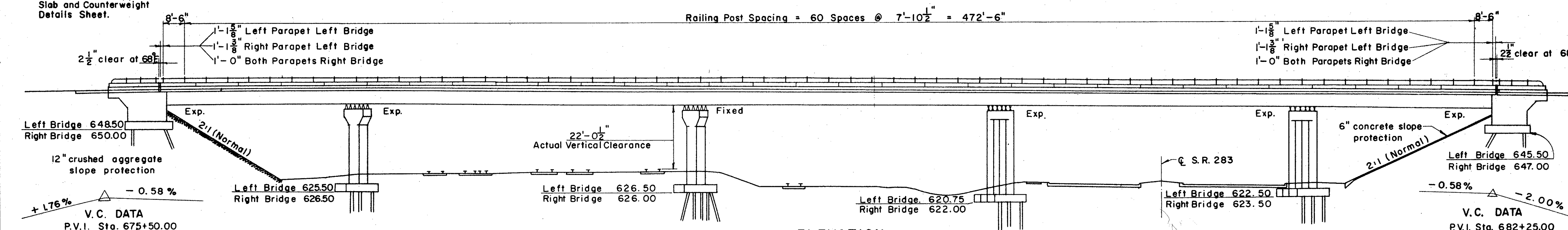
MACHINE FINISH: The bridge deck slabs shall be machine finished (Sec. 5-1.23)



GENERAL PLAN

Counterweight each end. For details see Approach Slab and Counterweight Details Sheet.

For scupper spacing see Framing Plan Details.



ELEVATION

NOTE: Elevation taken along Right Parapet of Right Bridge.

GENERAL NOTES

PROCEDURE: The embankment shall be placed and compacted up to the finished spill-thru slope and to the level of the subgrade for a distance of 200' back of the abutments, after which a delay of 30 days shall occur before excavation for the abutments is made. All piling shall be driven before any footings are placed.

V.C. DATA
P.V.I. Sta. 675+50.00
Elev. = 665.86'
L.V.C. = 650.00'
M.O. = 1.90'

V.C. DATA
P.V.I. Sta. 682+25.00
Elev. = 661.95'
L.V.C. = 700.00'
M.O. = 1.24'

PREPARED BY
CAPITOL ENGINEERING ASSOCIATES, DILLSBURG, PA.
FOR

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
DIVISION OF DESIGN AND CONSTRUCTION
BUREAU OF BRIDGES

GENERAL PLAN
BRIDGE NO. LAK-2-1471 L & R
RELOC. S. R. 2 OVER B.&O. R.R.-S. R. 283
LAKE COUNTY
STA. 680 + 56.97

DESIGNED FCM	DRAWN HMS	TRACED JZZ	CHECKED JCM DRT	REVIEWED DATE	REVISION
-----------------	--------------	---------------	-----------------------	---------------	----------