

NOTE:  
Remove Depressed Curb and Replace with Type 6 Curbs as shown on plan.

Remove Sewer Connections along St. Clair Street (See Note)

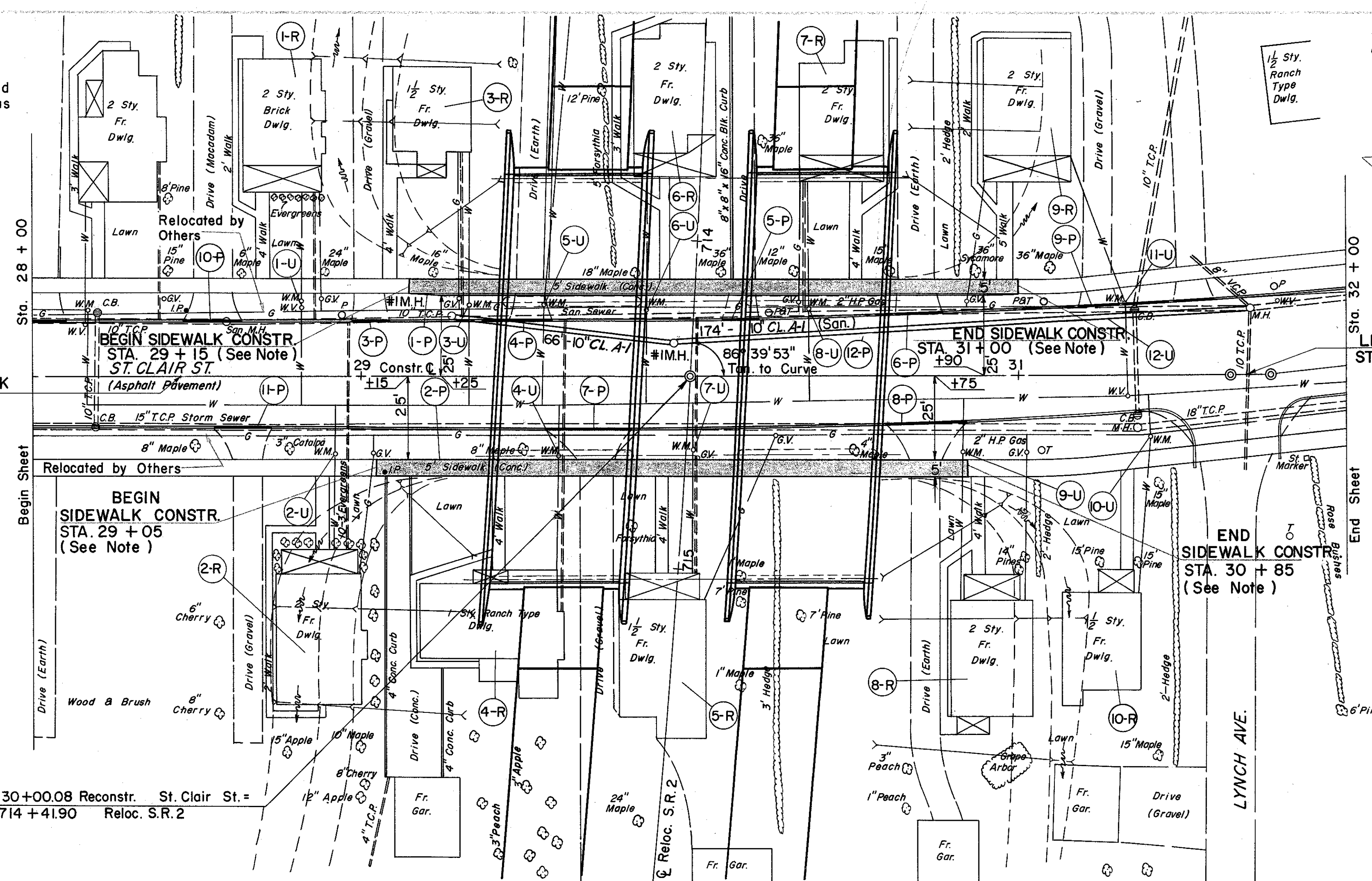
Telephone Line Relocated by Others.

FOR POWER ADJUSTMENTS SEE SHEET NO. 286

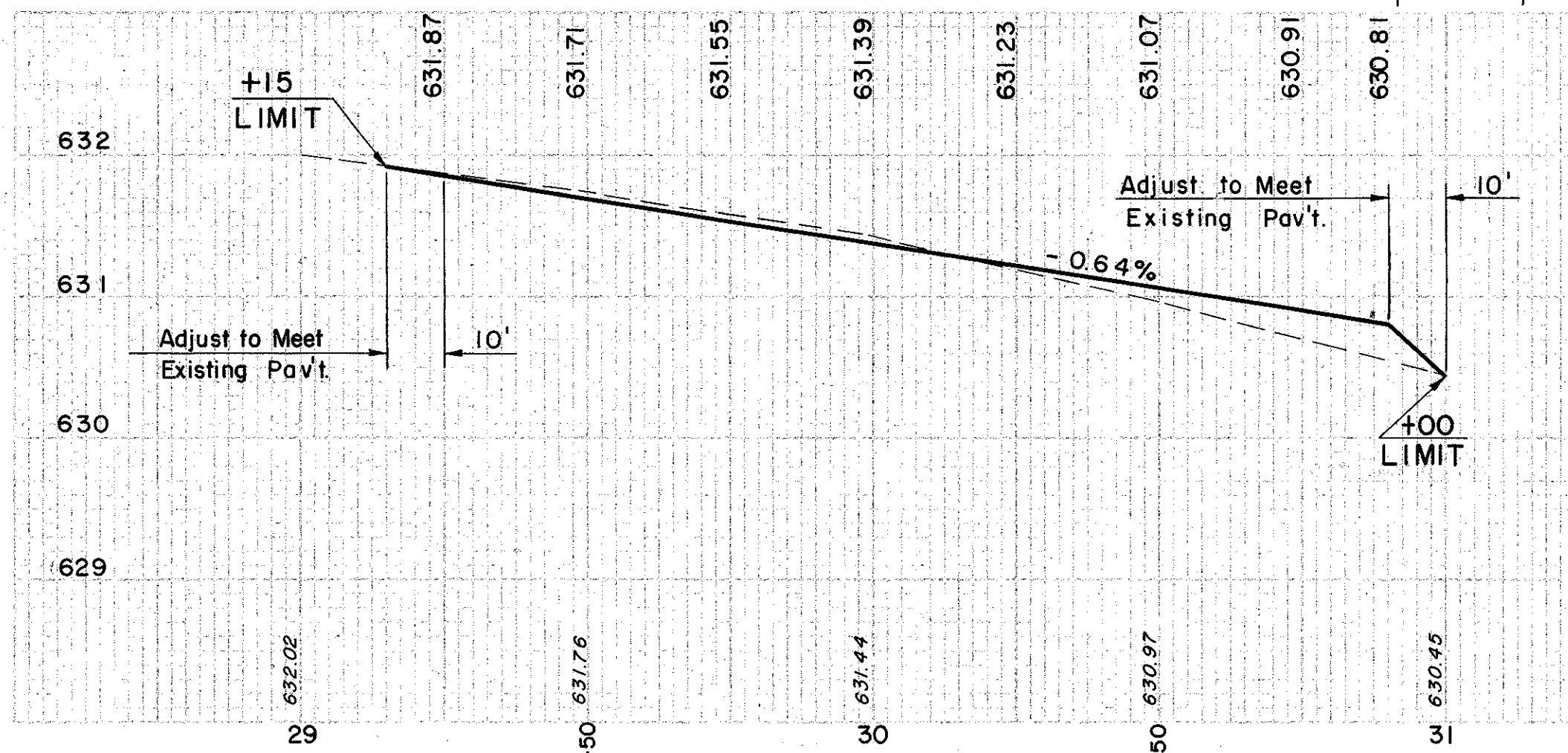
BRIDGE NO. LAK-2-1541 FOR STRUCTURE DETAILS SEE SHEETS NO. 331-338

LIMIT OF WORK STA. 28+45

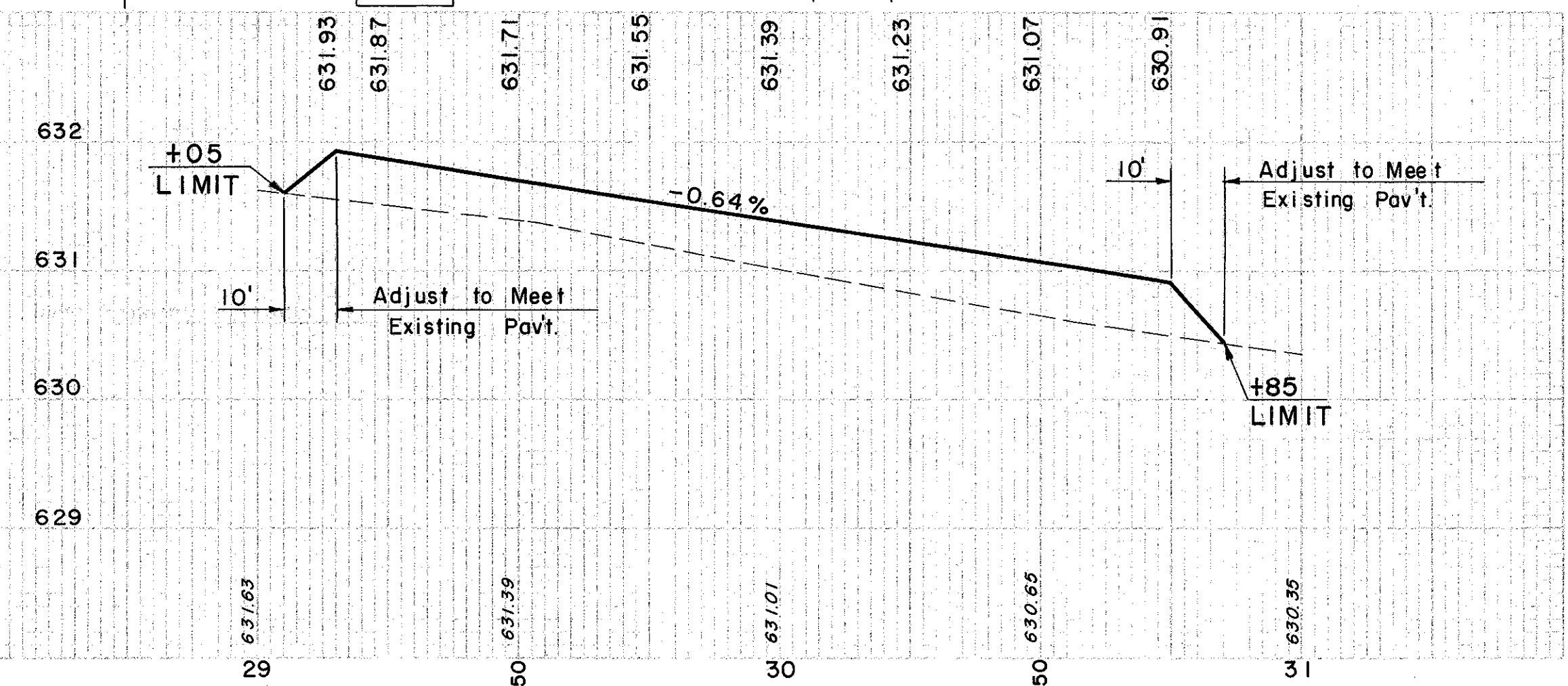
LIMIT OF WORK STA. 31+70



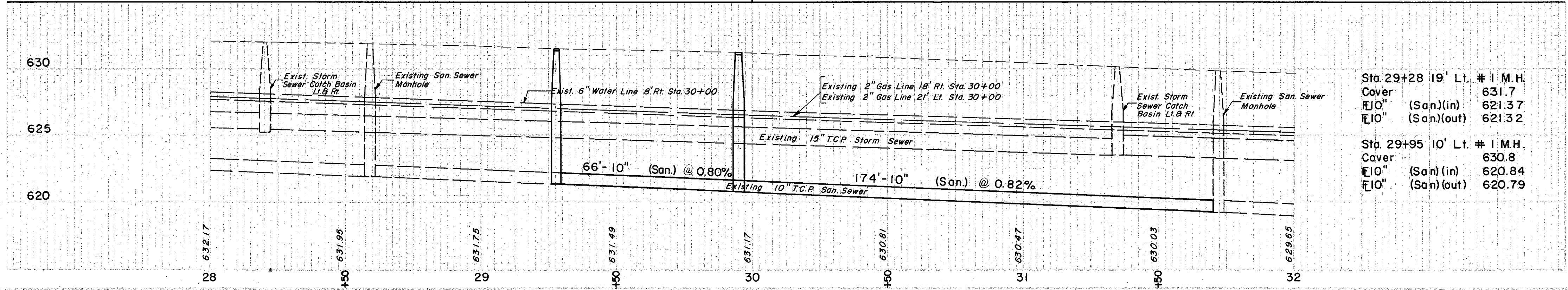
Sta. 30+00.08 Reconstr. St. Clair St. = Sta. 714 + 41.90 Reloc. S.R. 2



PROFILE ALONG INSIDE EDGE OF LEFT SIDEWALK



PROFILE ALONG INSIDE EDGE OF RIGHT SIDEWALK



Sta. 29+28 19' Lt. # 1 M.H. Cover	631.7
℄10" (San)(in)	621.37
℄10" (San)(out)	621.32
Sta. 29+95 10' Lt. # 1 M.H. Cover	630.8
℄10" (San)(in)	620.84
℄10" (San)(out)	620.79

(R) ROADWAY

REF. NO.	STATION	SIDE	E-4 GRANULAR BORROW	
			FROM	TO
1-R	28+75	LT.		199
2-R	28+85	RT.		270
3-R	29+20	LT.		168
4-R	29+40	RT.		309
5-R	29+90	RT.		279
6-R	29+90	LT.		245
7-R	30+50	LT.		204
8-R	30+90	RT.		224
9-R	31+00	LT.		227
10-R	31+20	RT.		249
TOTAL				2,374

(P) PAVEMENT

REF. NO.	STATION	SIDE	I-12	I-13	T-35	T-30	B-19
			CONC. CURB TYPE 6	CONC. SIDE WALK 4"	ASPH. CONC. SURF. COURSE 2"	BIT. PRIME COAT	AGGR. BASE COURSE
	FROM	TO	L.F.	S.F.	C.Y.	GAL.	C.Y.
1-P	29+15	31+00	LT.	925			
2-P	29+05	30+85	RT.	900			
3-P	28+97	29+09	LT.	12			
4-P	29+45	29+59	LT.	36			
5-P	30+10	30+22	LT.	12			
6-P	30+63	30+78	LT.	15			
7-P	29+56	29+83	RT.	27			
8-P	30+59	30+84	RT.	25			
9-P	31+18	31+31	LT.	13			
10-P	28+45	28+63	LT.	18			
11-P	28+55	28+73	RT.	18			
12-P	29+28	31+70	LT.		6	43	24
TOTAL			176	1,825	6	43	24

(U) UTILITIES

REF. NO.	STATION	SIDE	I-1	I-8	I-24.08	I-24.11
			SAN. SWR. CLASS A-1 M.6.8.(b) 10"	STD. #1 MANHOLE	SERVICE STOPS & BOXES REMOVED	WATER METERS & BOXES REMOVED
	FROM	TO	L.F.	EA.	EA.	EA.
1-U	28+82	LT.				
2-U	28+92	RT.				
3-U	29+33	LT.				
4-U	29+60	RT.				
5-U	29+55	LT.				
6-U	29+86	LT.				
7-U	30+00	RT.				
8-U	30+36	LT.				
9-U	30+82	RT.				
10-U	31+39	RT.				
11-U	31+33	LT.				
12-U	29+28	31+70	LT.	240	2	
TOTAL			240	2	11	11

EXCAVATION	0 C.Y.
EMBANKMENT	0 C.Y.
EMBANKMENT 20%	0 C.Y.