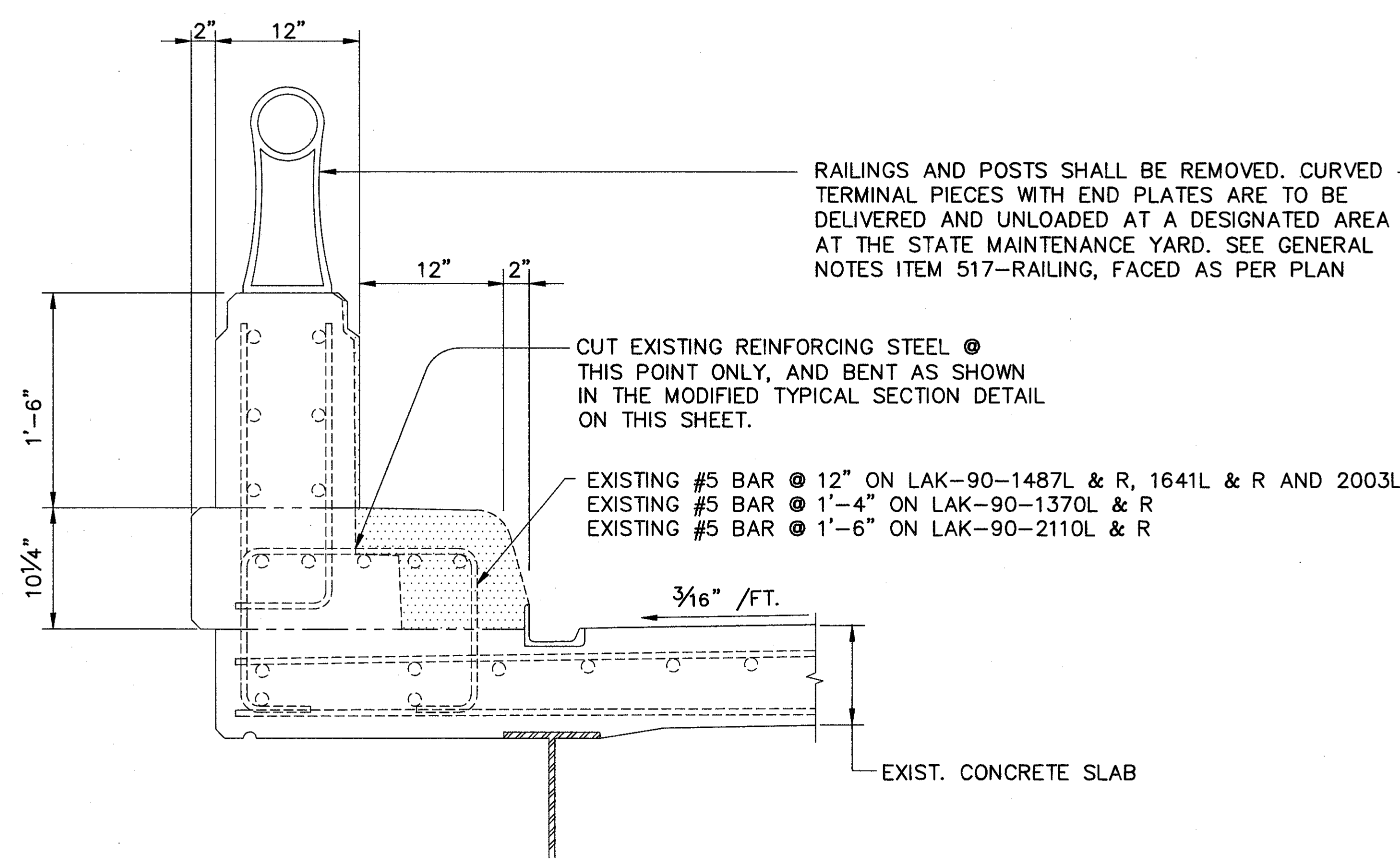


LAK-90-8.77/29.20



RAILINGS AND POSTS SHALL BE REMOVED. CURVED TERMINAL PIECES WITH END PLATES ARE TO BE DELIVERED AND UNLOADED AT A DESIGNATED AREA AT THE STATE MAINTENANCE YARD. SEE GENERAL NOTES ITEM 517-RAILING, FACED AS PER PLAN

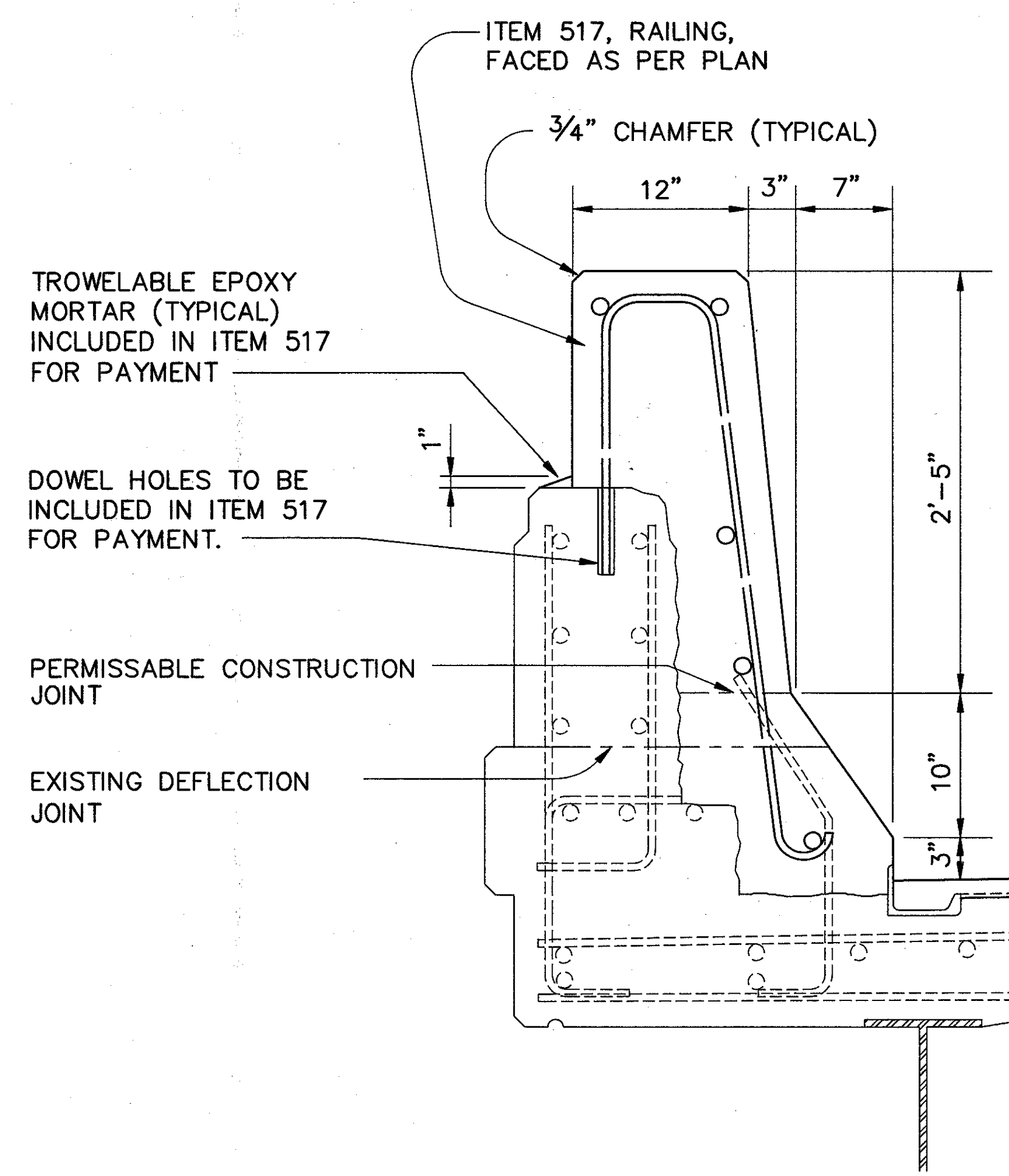
CUT EXISTING REINFORCING STEEL @ THIS POINT ONLY, AND BENT AS SHOWN IN THE MODIFIED TYPICAL SECTION DETAIL ON THIS SHEET.

EXISTING #5 BAR @ 12" ON LAK-90-1487L & R, 1641L & R AND 2003L & R  
 EXISTING #5 BAR @ 1'-4" ON LAK-90-1370L & R  
 EXISTING #5 BAR @ 1'-6" ON LAK-90-2110L & R

INDICATES REMOVAL AREA (RETAIN AND BEND EXISTING STEEL AS SHOWN IN MODIFIED TYPICAL SECTION. WHERE SALVAGE OF AN EXISTING BAR IS NOT PRACTICABLE, NEW REINFORCING STEEL IS TO BE USED. SEE GENERAL NOTES, ITEM 509-REINFORCING STEEL, GRADE 60, AS PER PLAN.)

(SEE OPPOSITE SIDE FOR EXIST. DIMENSIONS)

**EXISTING TYPICAL SECTION**



**NOTES**

ALL TRANSVERSE STEEL SHALL BE SPACED 12" CENTER TO CENTER UNLESS SHOWN OTHERWISE

FOR LOCATION OF DEFLECTION JOINTS SEE SHEETS 9/37, 10/37, 12/37, 14/37 AND 15/37 & BR-1. DEFLECTION JOINTS TO MATCH EXISTING.

LONGITUDINAL REINFORCING STEEL SHALL END 2 INCH MINIMUM TO 3 INCH MAXIMUM FROM ALL JOINTS

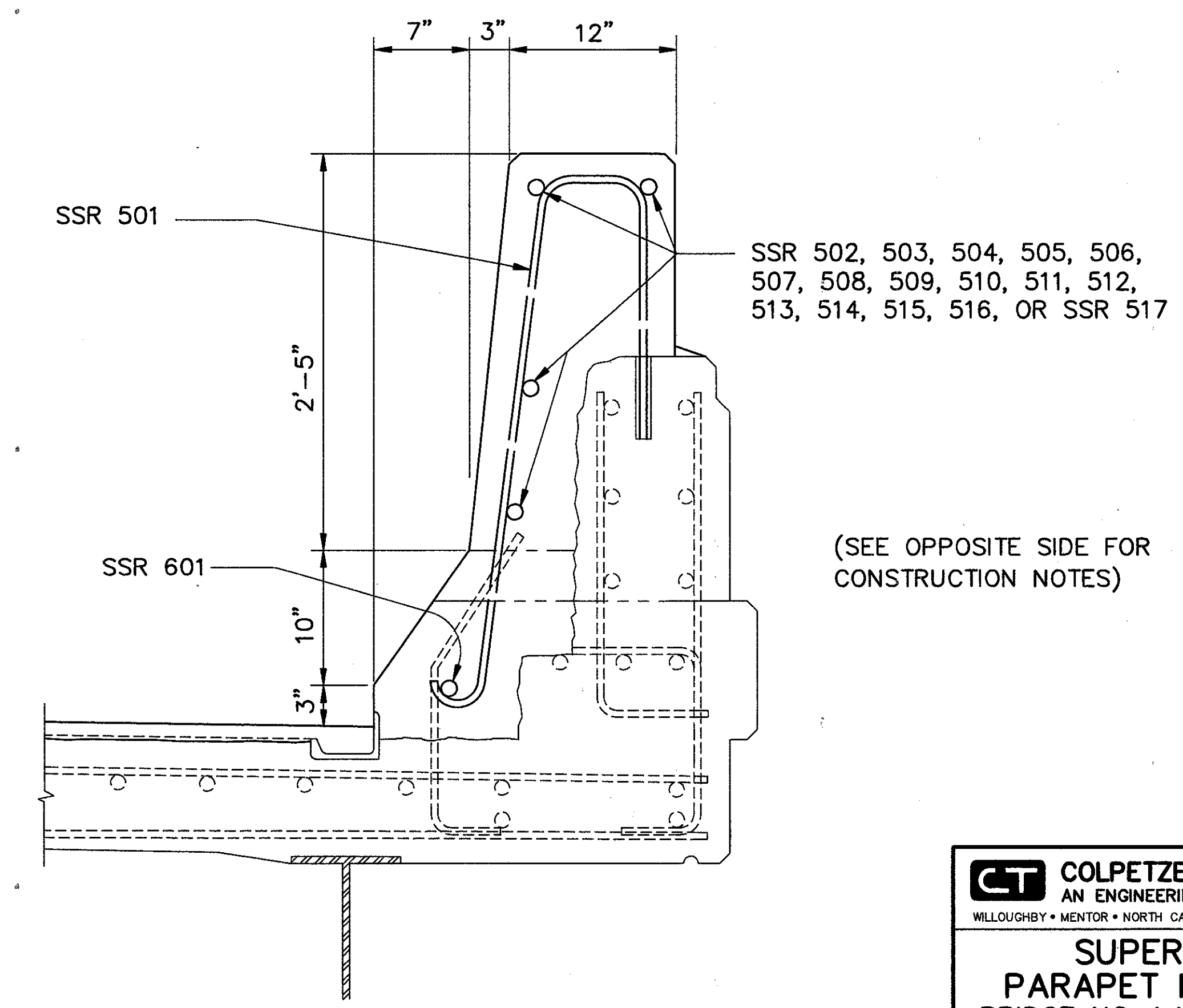
CONCRETE COVER SHALL BE 2 INCH TYPICAL

SEE SHEET 31/37 FOR DETAILS OF MODIFICATIONS OF EXPANSION JOINTS

FOR ADDITIONAL NOTES AND INFORMATION SEE BR-1 AND THE GENERAL NOTES, ITEM 517-RAILING, FACED AS PER PLAN.

1/4" LATEX MODIFIED CONCRETE OVERLAY (TYPICAL)

**MODIFIED TYPICAL SECTION**



(SEE OPPOSITE SIDE FOR CONSTRUCTION NOTES)

<b>COLPETZER-THOMAS, INC.</b> AN ENGINEERING GROUP <small>WILLOUGHBY • MENTOR • NORTH CANTON • STEUBENVILLE • LORAIN</small>						6 / 37
<b>SUPERSTRUCTURE          PARAPET MODIFICATIONS</b> BRIDGE NO. LAK-90-1370L/1370R, 1487L/1487R, 1641L/1641R, 2003L/2003R AND 2110L/2110R						
DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED	DATE	REVISED
J.E.A.	R.L.B.	R.L.B.	R.J.C.	B.J.A.	11/2/87	