

FINAL SURVEYED, PLOTTED, NOTE BOOK, AREAS CHECKED.

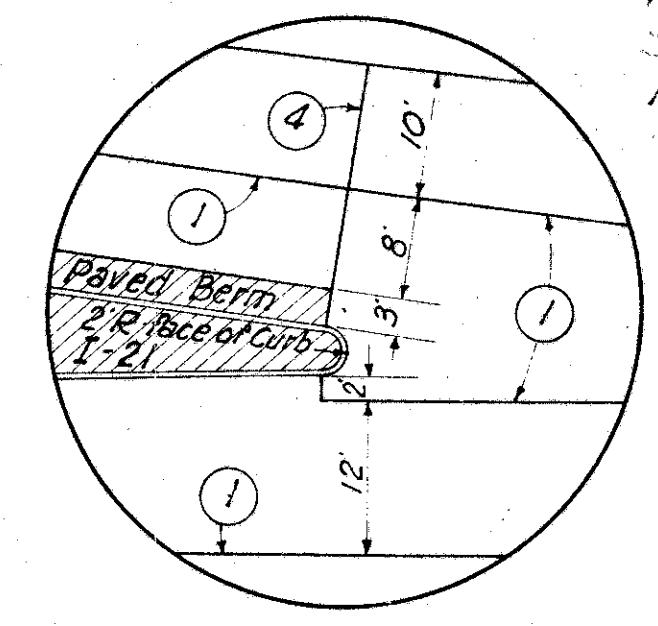
ORIGINAL SURVEYED, PLOTTED, NOTE BOOK, AREAS CHECKED.

FED. RD. DIVISION	STATE	PROJECT	
2	OHIO	1-1103(20) S-46(7)	190-339

LAKE COUNTY  
LAK-1-4.02

This sheet supersedes Sheet No. 190 Added 3-24-60 REC.

- LEGEND
- ① Standard Longitudinal Joint
  - ② Standard Key Joint Without Tie Bars
  - ③ Expansion Joint without Dowels (Loc. on radial lines)
  - ④ Expansion Joint with Dowels.

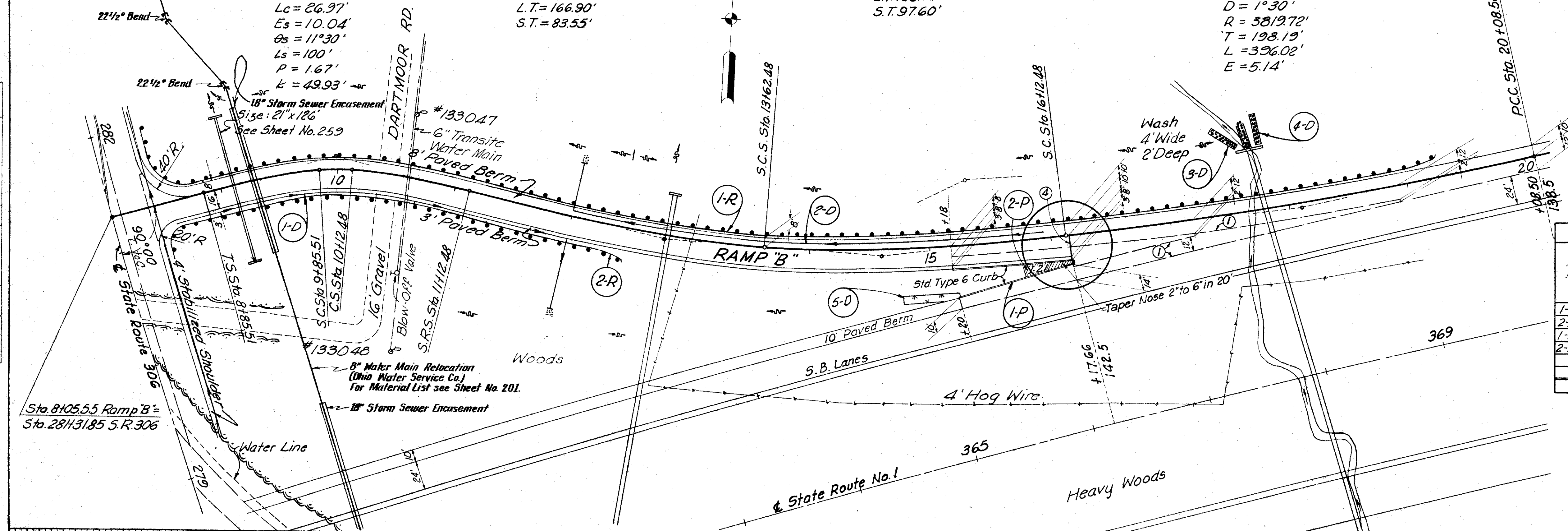


P.I. Sta. 10100.77  
 $\Delta = 29^\circ 12' 09''$   
 $D_c = 23^\circ 00'$   
 $R_c = 249.11'$   
 $T_s = 115.26'$   
 $L_c = 26.97'$   
 $E_s = 10.04'$   
 $\theta_s = 11^\circ 30'$   
 $L_s = 100'$   
 $P = 1.67'$   
 $k = 49.93'$

P.I. Sta. 1217938  
 $L_s = 250'$   
 $\theta_s = 9^\circ 22' 30''$   
 $P = 3.41'$   
 $K = 124.89'$   
 $L.T. = 166.90'$   
 $S.T. = 83.55'$

P.I. Sta. 14160.08  
 $L_s = 250'$   
 $\theta_s = 11^\circ 15' 00''$   
 $P = 2.73'$   
 $L.T. = 153.20'$   
 $S.T. = 97.60'$

P.I. Sta. 18110.67  
 $\Delta = 5^\circ 56' 25''$   
 $D = 1^\circ 30'$   
 $R = 3819.72'$   
 $T = 198.19'$   
 $L = 396.02'$   
 $E = 5.14'$



Ref. No.	Station		Side &	I-15 Guard Rail Dwg. No. 2-B Lin. Ft.	I-12 Conc. Curb Type 6 Lin. Ft.	I-21 Conc. Median Pavement Type 1 Sq. Yd.
	From	To				
1-R	8+47.55	19+24.75	Lt.	1100.0		
2-R	8+47.55	12+43.03	Rt.	401.0		
1-P	15+18	366+19.66	Rt.		200	
2-P	15+72.48	366+19.66	Rt.			28
Totals				1501.0	200	28

Sta. Ramp	Offset	Elev.	Sta. S.R.#1	Offset	Elev.
16+25		730.72	370+50	138.50	737.97
+50		731.15			738.38
+75		731.57	371+00		738.78
17+00		732.02	+25		739.19
+25		732.47	-50		739.60
-50		732.91	+75		740.01
+75		733.36	372+00		740.42
18+00		733.79	+25		740.83
+25		734.22	-50		741.23
+50		734.66	+75		741.64
+75		735.08	373+00		742.05
19+00		735.50	+25		742.46
+25		735.93	-50		742.87
-50		736.34	+75		743.28
+75		736.75	374+00		743.69
20+00		737.15	+25		744.09
20+08.50 BK			-50		744.50
370+08.50 AK	138.50	737.29	+75		744.91
+25	138.50	737.56	375+00	138.50	745.32

STA.	PG. on @	RT. EDGE	STA.	PG. on @	RT. EDGE
8+85.51	735.67	735.67	12+50	728.83	729.36
9+00	735.28	735.24	+75	728.64	729.26
+25	734.63	734.53	13+00	728.49	729.21
+50	734.02	733.86	+25	728.39	729.21
+75	733.45	733.23	+50	728.34	729.25
+85.51	733.22	732.97	+62.48	728.33	729.29
10+00	732.91	732.66	+75	728.33	729.25
+12.48	732.65	732.40	14+00	728.37	729.22
+25	732.41	732.19	+25	728.46	729.24
+50	731.92	731.76	+50	728.59	729.30
+75	731.43	731.34	+75	728.76	729.40
11+00	730.94	730.91	15+00	728.99	729.56
+12.48	730.70	730.70	+25	729.26	729.77
+25	730.47	730.52	+50	729.57	730.03
+50	730.05	730.19	+75	729.93	730.33
+75	729.68	729.92	16+00	730.34	730.68
12+00	729.35	729.69	+12.48	730.55	730.85
+25	729.06	729.49			

Station	From	To	Side	Station	From	To	Side	Total
1-4	8+57	12+00	Rt.	1-4	8+57	12+00	Lt.	806
1-5	13+00	18+00	Rt.	1-5	13+00	18+00	Lt.	24
1-10	19+00	24+00	Rt.	1-10	19+00	24+00	Lt.	11
Total								841

REVISIONS: 5-27-59 Water Main Relocated at Sta. 12+93.30  
 3-18-60 Grade Raise for 16'-4" Br. Cl.

MICHAEL BAKER JR., CONSULTING ENGINEERS  
 ROCHESTER, PENNSYLVANIA

STATE OF OHIO  
 DEPARTMENT OF HIGHWAYS  
 STATE HIGHWAY NO. 1  
 C-44  
 SR. NO. 306 INTERCHANGE RAMP B  
 STA. 8+00 TO STA. 20+00

Designed	Drawn	Traced	Checked	Reviewed Date	Revised
H.V.	R.P.	K.A.	J.H.		

