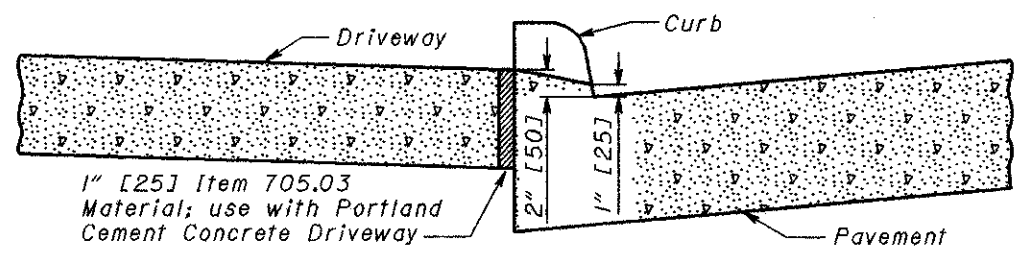
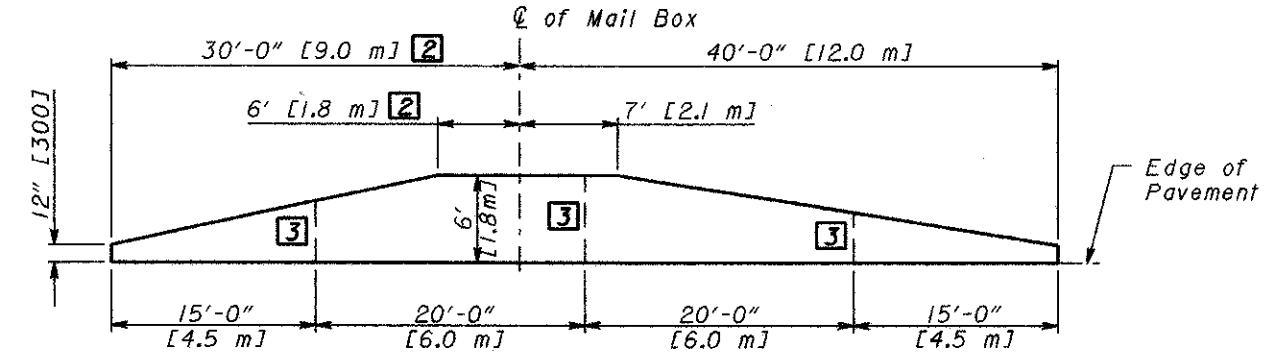
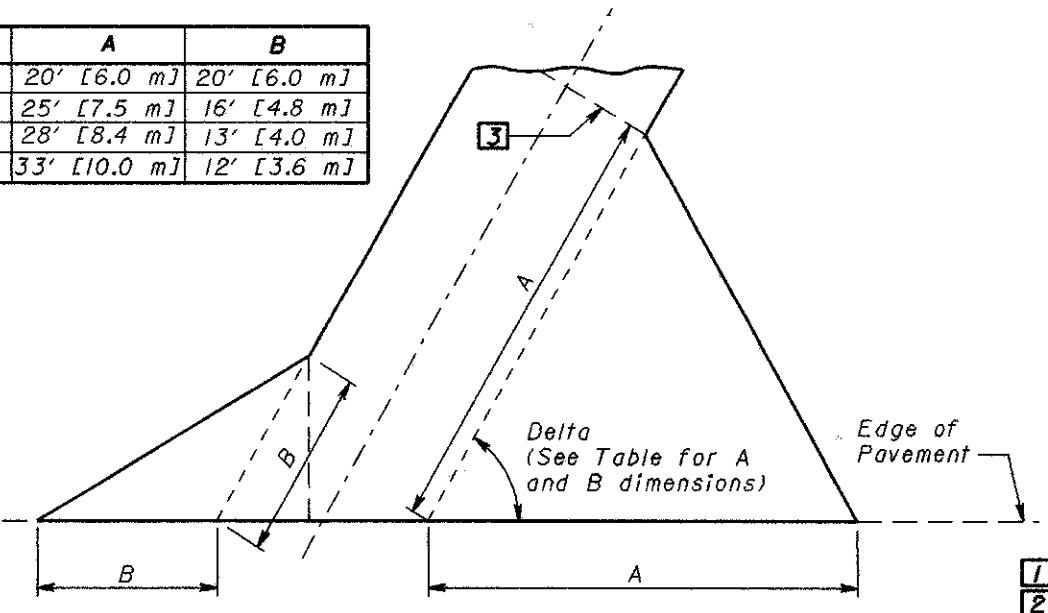
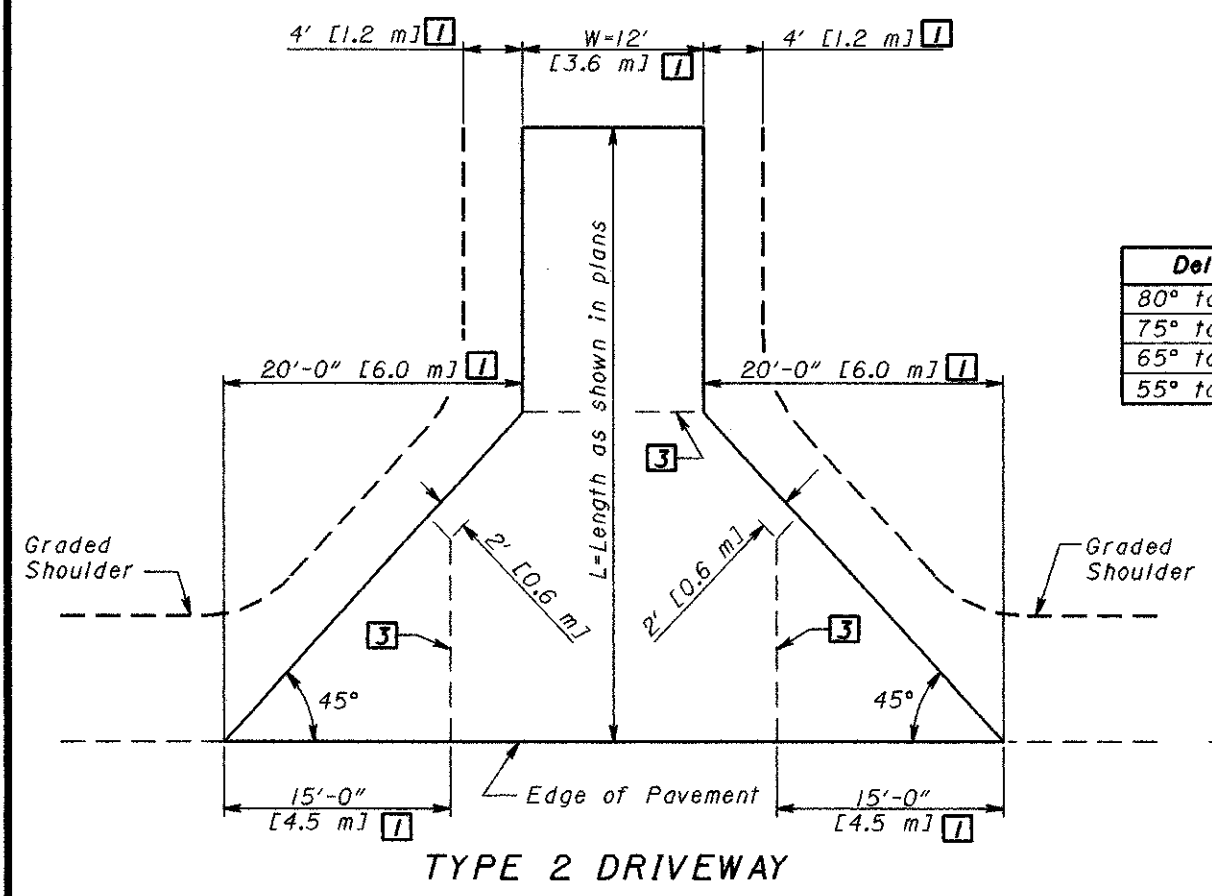


**COMBINED DRIVEWAY & MAIL BOX APPROACH**



**DROP CURB DETAILS AT DRIVEWAYS**

Delta	A	B
80° to 90°	20' [6.0 m]	20' [6.0 m]
75° to 85°	25' [7.5 m]	16' [4.8 m]
65° to 75°	28' [8.4 m]	13' [4.0 m]
55° to 65°	33' [10.0 m]	12' [3.6 m]



Transition from Standard Curb Section to Drop Curb Section to be made in 18" [450] distance from Driveway

**NOTES**

- GENERAL:** The design details shown here shall govern the construction of driveways unless otherwise shown in the project plans.
- The pavement type and thickness shall be specified in the project plans.
- Driveway and mail box approaches shall be combined when feasible.
- JOINTS:** Impressed joints for portland cement concrete driveways shall be 1/4" [6] minimum width by 3"± [75±] depth and shall be sealed with Item 705.04 or ASTM D 1850.
- In addition to the joints shown here, impressed joints without tie bars shall be placed in portland cement concrete driveways at intervals not to exceed 17' [5.2 m] in the portion of the driveway beyond the flare.

**LEGEND**

- 1 Unless otherwise shown in the plans.
- 2 Add 3' [0.9 m] for each additional Mail Box
- 3 Impressed Joint without Tie Bars for Portland Cement Surface

THIS DRAWING REPLACES BP-4.1M DATED 10-28-94.  
 STANDARD ROADWAY CONSTRUCTION DRAWING  
**DRIVEWAYS**  
 NUMBER **BP-4.1**  
 REVISIONS  
 M. EVANS  
 D. FOCKE  
 ROADWAY DESIGN ENGINEER  
 DATE