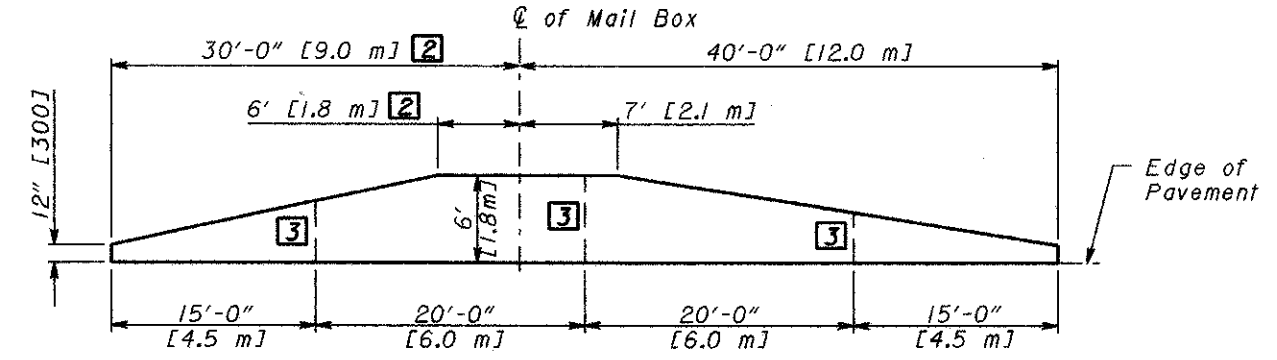
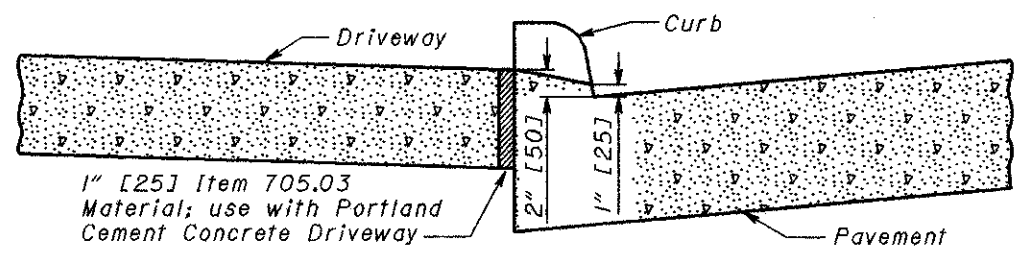


**COMBINED DRIVEWAY & MAIL BOX APPROACH**

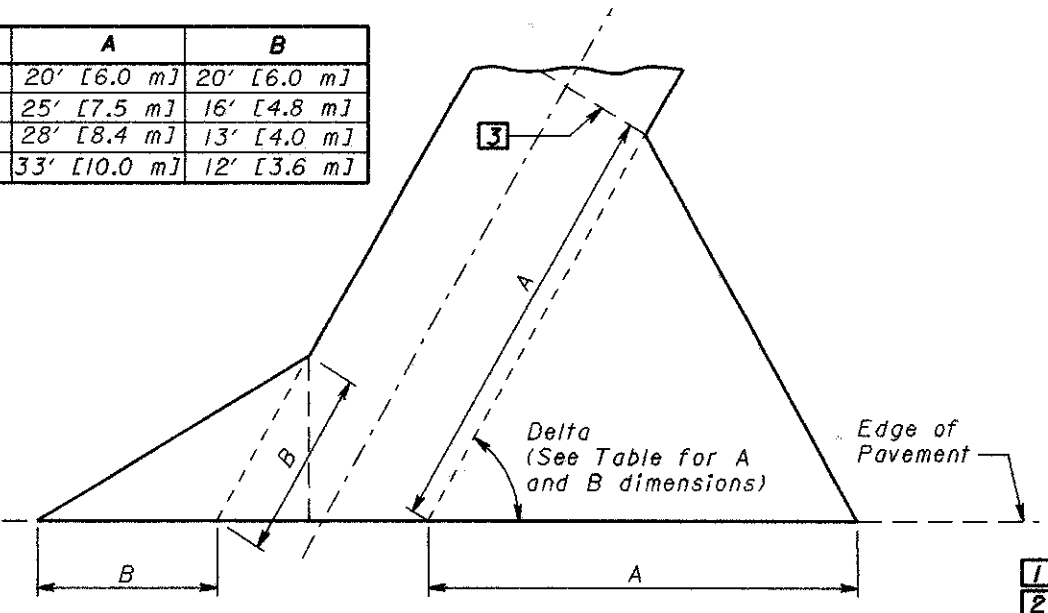
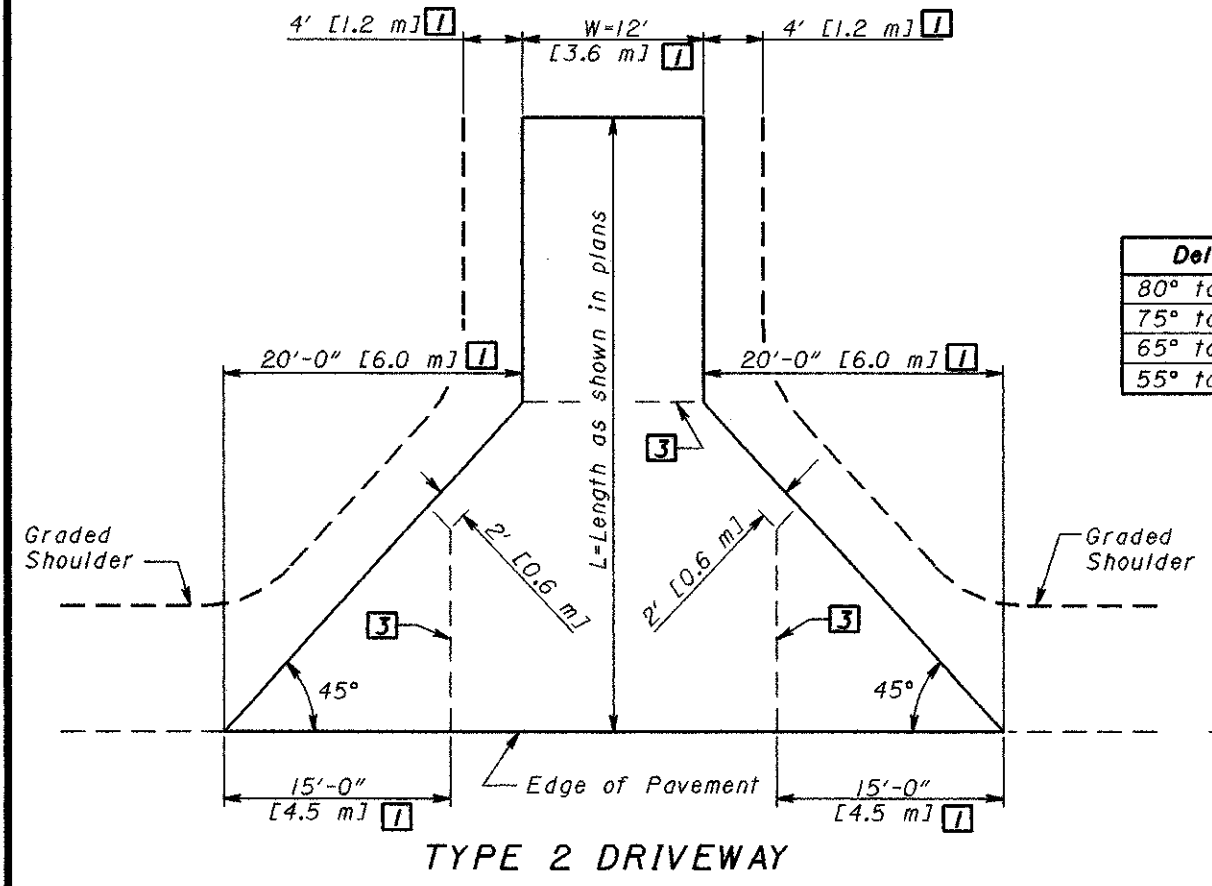


**TYPICAL MAIL BOX APPROACH**



**DROP CURB DETAILS AT DRIVEWAYS**

| Delta      | A            | B           |
|------------|--------------|-------------|
| 80° to 90° | 20' [6.0 m]  | 20' [6.0 m] |
| 75° to 85° | 25' [7.5 m]  | 16' [4.8 m] |
| 65° to 75° | 28' [8.4 m]  | 13' [4.0 m] |
| 55° to 65° | 33' [10.0 m] | 12' [3.6 m] |



Transition from Standard Curb Section to Drop Curb Section to be made in 18" [450] distance from Driveway

**NOTES**

- GENERAL:** The design details shown here shall govern the construction of driveways unless otherwise shown in the project plans.
- The pavement type and thickness shall be specified in the project plans.
- Driveway and mail box approaches shall be combined when feasible.
- JOINTS:** Impressed joints for portland cement concrete driveways shall be 1/4" [6] minimum width by 3"± [75±] depth and shall be sealed with Item 705.04 or ASTM D 1850.
- In addition to the joints shown here, impressed joints without tie bars shall be placed in portland cement concrete driveways at intervals not to exceed 17' [5.2 m] in the portion of the driveway beyond the flare.

**LEGEND**

- 1 Unless otherwise shown in the plans.
- 2 Add 3' [0.9 m] for each additional Mail Box
- 3 Impressed Joint without Tie Bars for Portland Cement Surface

THIS DRAWING REPLACES BP-4.1M DATED 10-28-94.  
 STANDARD ROADWAY CONSTRUCTION DRAWING  
 ROADWAY ENGINEERING SERVICES  
 DEPARTMENT OF TRANSPORTATION  
 REVISIONS  
 STDS. ENGR. M. EVANS  
 DRAWN D. FOCKE  
 DATE  
 11/11