

TYPICAL SECTIONS

FED. RD. DIST. NO.	STATE	FEDERAL AID PROJECT	FISCAL YEAR	3 64
10	OHIO	5-A(4)	1940	

LAKE COUNTY
S.H. 2 SEC. Q (PT.)

TRAFFIC NOTE

SEE SHEET No. 62

GENERAL NOTES

SUPERELEVATED CURVES SHALL BE BUILT WITHOUT CROWN. THE CROWN SHALL BE WORKED OUT OF THE PAVEMENT IN THAT PORTION BETWEEN THE BEGINNING OF THE TRANSITION AND THE POINT WHERE THE SUPERELEVATION EQUALS TWICE THE CROWN.

THE THICKNESS OF THE SUBGRADE IN CUTS TO BE COMPACTED ON THIS PROJECT WILL BE EIGHT INCHES LOOSE MEASUREMENT. THE WIDTH SHALL BE THE WIDTH OF THE PAVEMENT PLUS EIGHTEEN INCHES ON EACH SIDE OF THE PAVEMENT.

ALL BERMS AND SLOPES SHALL BE FINISHED IN ACCORDANCE WITH THE TYPICAL SECTIONS EXCEPT WHERE OTHERWISE SHOWN ON THE CROSS-SECTIONS.

COMPACTION WILL NOT BE REQUIRED ON THE TOP 3' OF EMBANKMENT DUE TO SEEDING.

ITEMS E-305 & L-9 SHALL BE PERFORMED ON ALL EARTH AREA WITHIN THE LIMITS OF THE WORK.

SEED SHALL BE SOWN AT THE RATE OF 4 LBS. PER 1000 SQ. FT. USING THE FOLLOWING MIXTURE: 60% KENTUCKY BLUE GRASS, 20% DOMESTIC RYE GRASS, & 20% RED TOP.

STRAW SHALL BE HELD IN PLACE BY THE USE OF LATH STAKES IN LENGTHS OF 4' TO 8'. TWINE SHALL CONNECT STAKES AND CROSS NEAR THE CENTER OF THE SQUARE OR RECTANGULAR AREA FORMED BY STAKE PLACEMENT. (SEE DIAGRAM BELOW)

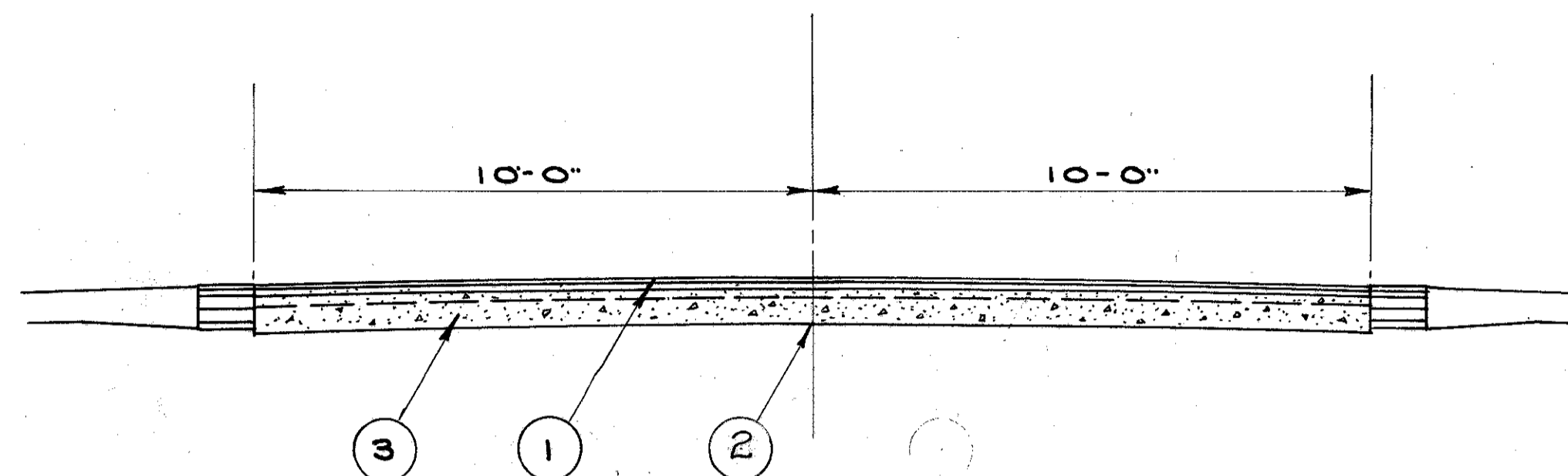
IN PRUNING EXISTING TREES TREATMENT OF CAVITIES WILL NOT BE REQUIRED.

WHERE OPENINGS ARE MADE ACROSS THE ROAD FOR SEWER CONNECTIONS, SAME SHALL BE IMMEDIATELY BACKFILLED AND PROTECTED IN ORDER TO PROVIDE THE MINIMUM OF INCONVENIENCE TO TWO-WAY TRAFFIC.

NO TREES SHALL BE REMOVED UNTIL SPECIFICALLY MARKED FOR REMOVAL BY THE ENGINEER.

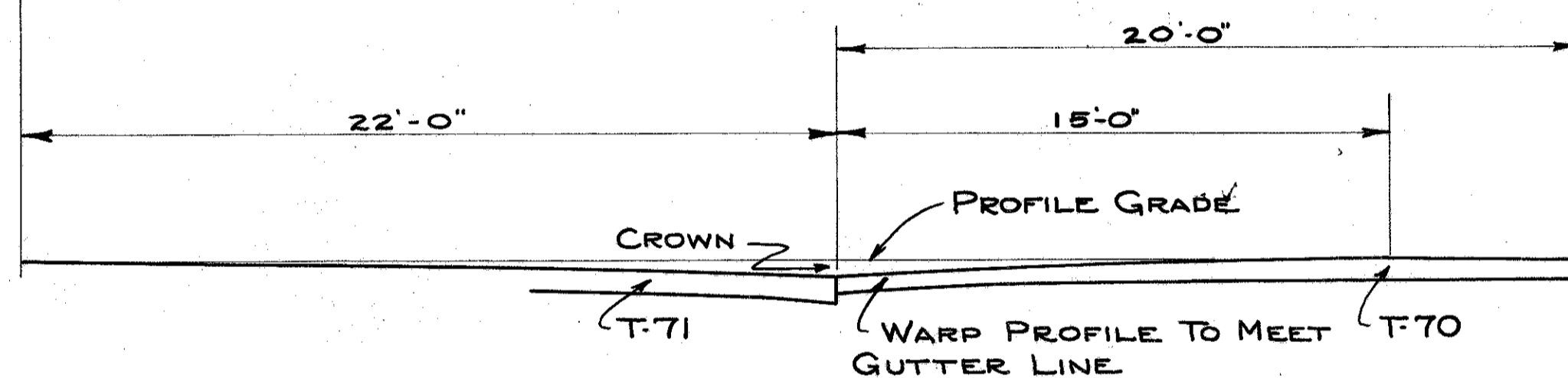
ALL EXISTING PAVEMENT WHICH IS TO BE REMOVED SHALL BE USED IN THE EMBANKMENTS TO REDUCE BORROW, OR THE CONTRACTOR MAY SUPPLY BORROW CLASSIFIED BY THE P.R.A. AS A-1, A-2, OR A-3 AT THE RATE OF 1.3 CU. YDS. FOR EACH CU. YD. OF PAVEMENT MEASURED IN ITS ORIGINAL POSITION, IN LIEU OF USING REMOVED PAVEMENT.

AGGREGATE FOR SIDE APPROACHES SHALL BE 60% NO. 4 AND 40% NO. 7 AND COMPACTED WITH ROLLER WEIGHING NOT LESS THAN 2 1/2 TONS.



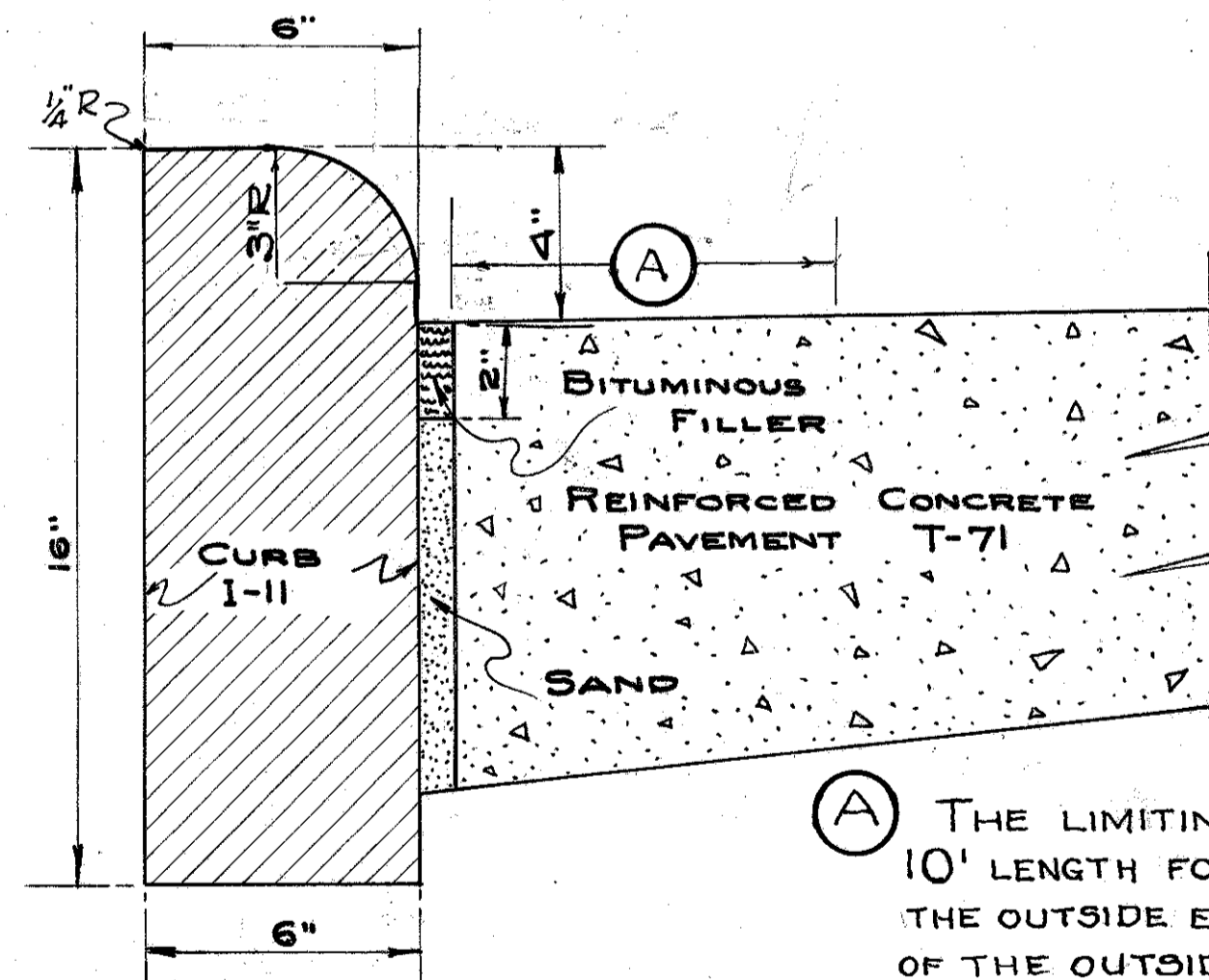
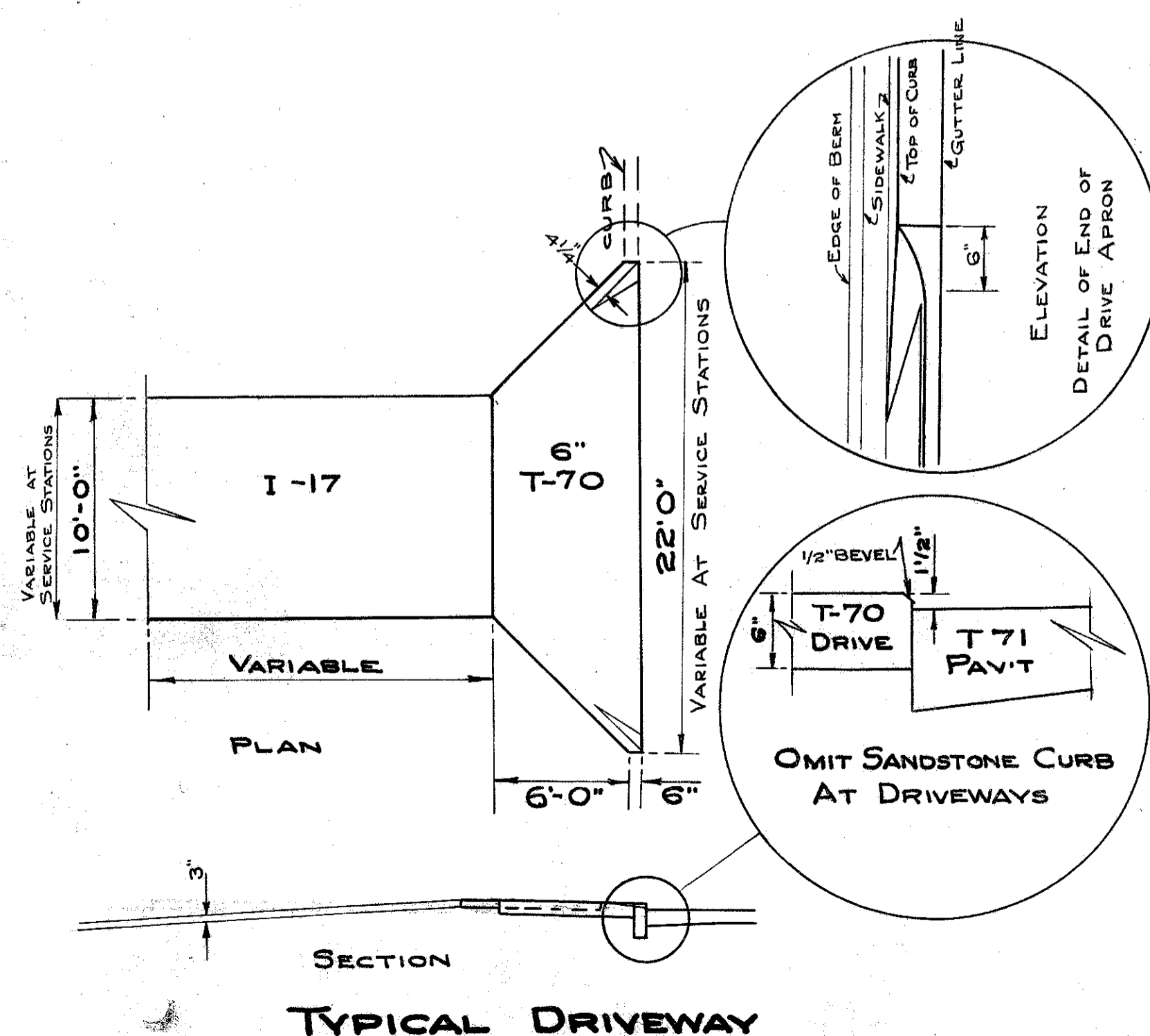
REPLACEMENT FOR DEFECTIVE AREAS AND CROSS-OVERS

- ① ITEM T-50 HOT-MIXED, HOT-LAID ASPHALTIC CONCRETE SURFACE COURSE - 1" TYPE "B" WEARING COURSE, 1 1/2" BINDER COURSE.
 - ② LONGITUDINAL JOINT
 - ③ ITEM B-71 8" REINFORCED PORTLAND CEMENT CONCRETE BASE COURSE.
- NOTE: FULL DEPTH WOOD FORMS AND FINISHING ACCORDING TO B-70.191 WILL BE PERMITTED ON 8" B-71 BASE COURSE.



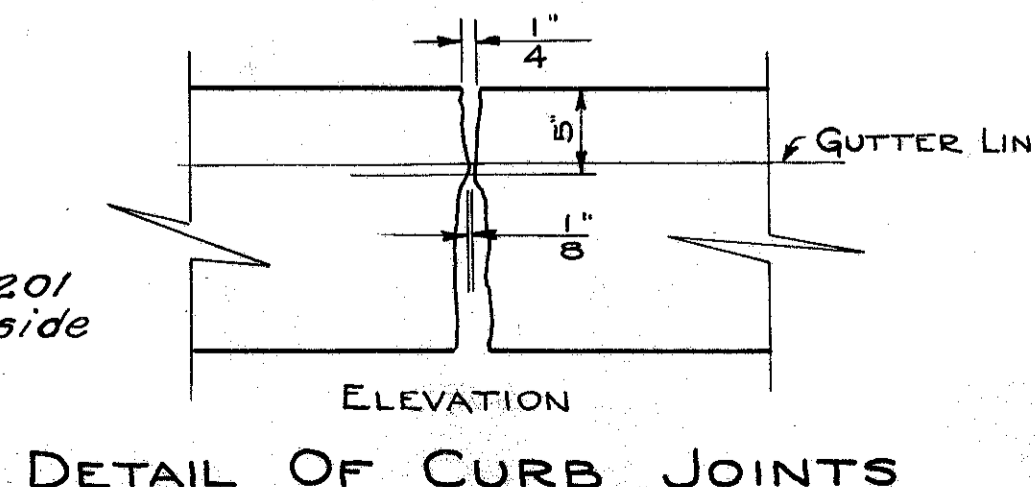
SCALE 1"=4'-0"

DETAIL OF SIDE ROAD APPROACHES AT GUTTER



DETAIL OF SANDSTONE CURB

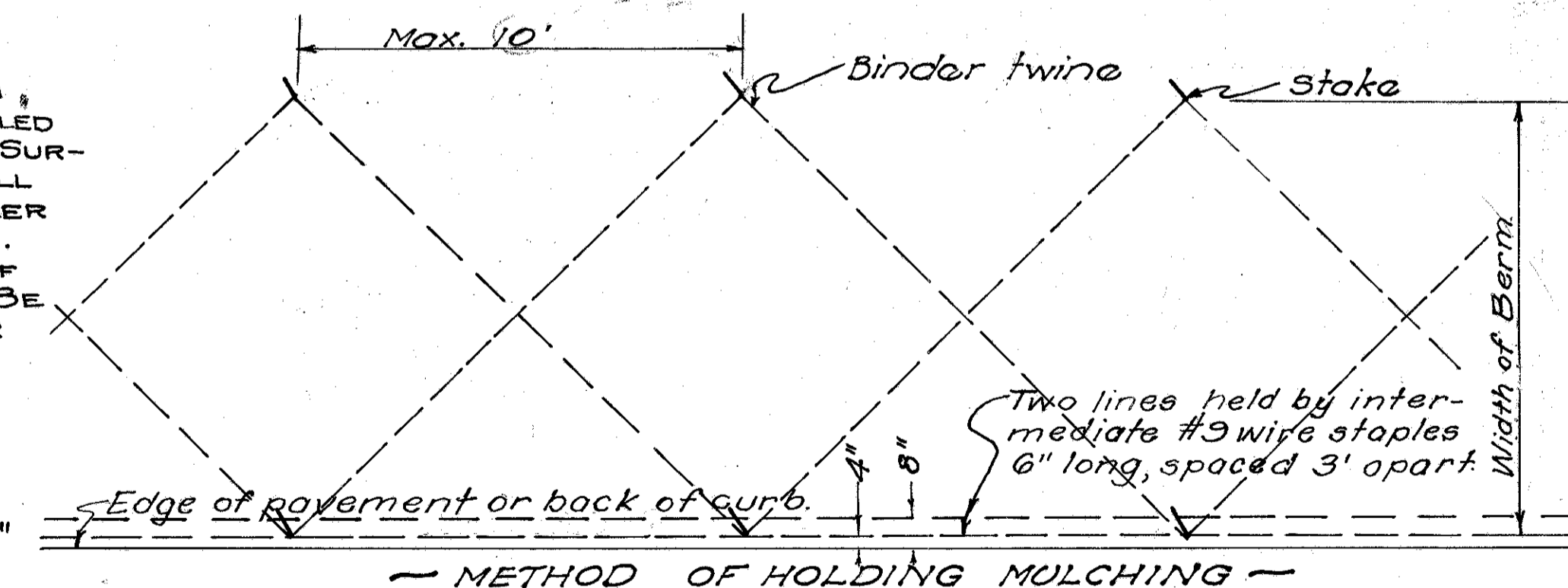
Note: Full depth wood forms may be used for 6" T-70 pavement on private drives. Finishing according to Sec. T-70.201 will be permitted on all pavement outside parallel lines 22' rt. & lth. of $\frac{1}{2}$.



DETAIL OF CURB JOINTS

PAVEMENT TO BE FULL WIDTH DISTANCE SHOWN ON TYPICAL SECTION. CURB SHALL BE OUTSIDE PAVEMENT WIDTH.

ANY OPENING REMAINING BETWEEN CURB AND PAVEMENT SHALL BE FILLED WITH DRY SAND TO WITHIN 2" OF SURFACE. THE REMAINING SPACE SHALL BE FILLED WITH BITUMINOUS FILLER MEETING REQUIREMENTS OF ITEM M-5.5 (F-2). SAND SHALL MEET REQUIREMENTS OF ITEM M-2.2. PAYMENT FOR JOINT TO BE INCLUDED IN UNIT PRICE BID PER LINEAL FOOT OF CURB.



Where directed, the above twine and pegs shall extend beyond the berm pegs to be spaced at 12' maximum.