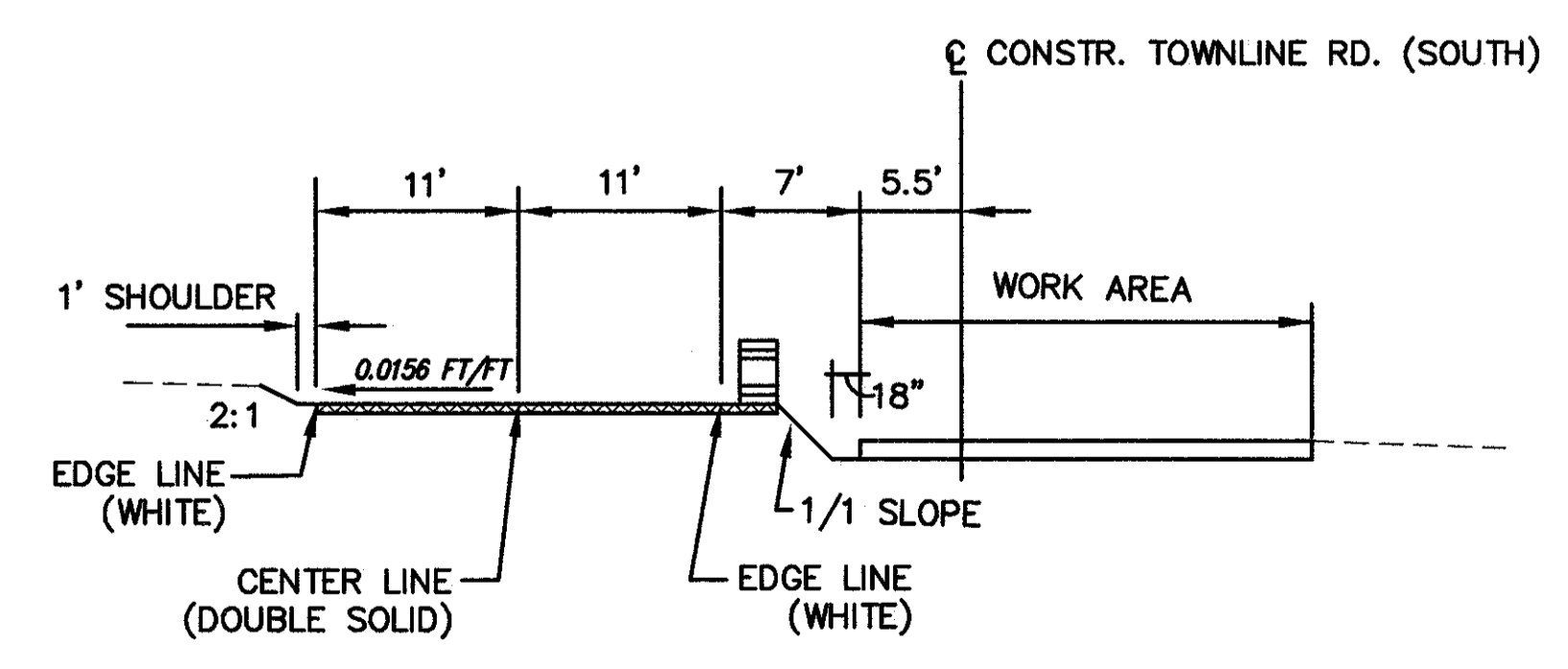
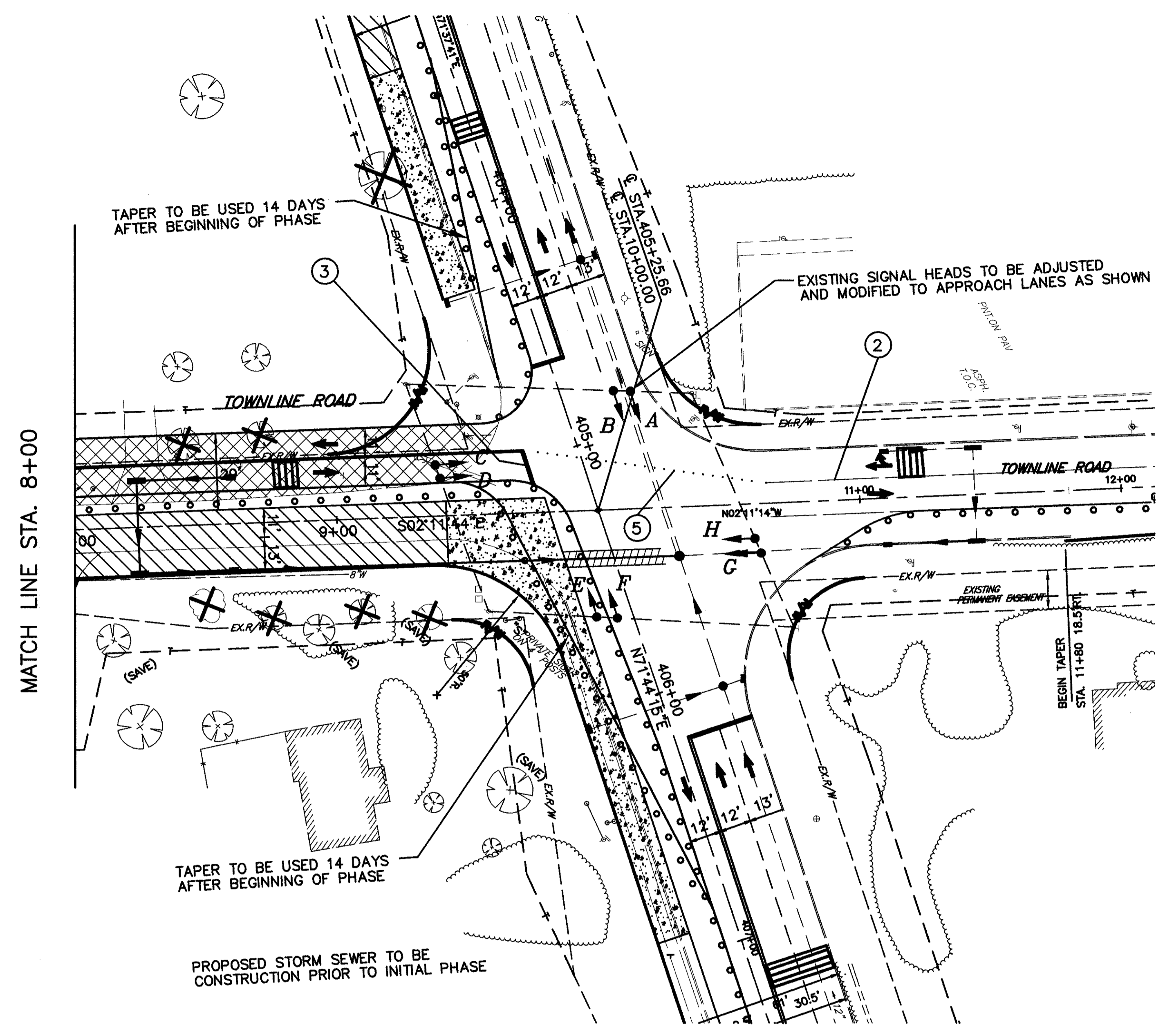


| FUNCTION | φ1 | φ3 | φ4 |
|---------------------------|-----|-----|-----|
| MINIMUM GREEN | 25 | 15 | 15 |
| VEHICLE EXTENSION | 3 | 3 | 3 |
| MAXIMUM GREEN | 40 | 30 | 30 |
| VEHICLE YELLOW CLEARANCE | 3.5 | 3.5 | 3.5 |
| VEHICLE ALL RED CLEARANCE | 1.0 | 1.0 | 1.0 |
| RECALL | MIN | | |
| MEMORY | ON | ON | ON |

SIGNAL TIMING CHART



PART A TYPICAL SECTION

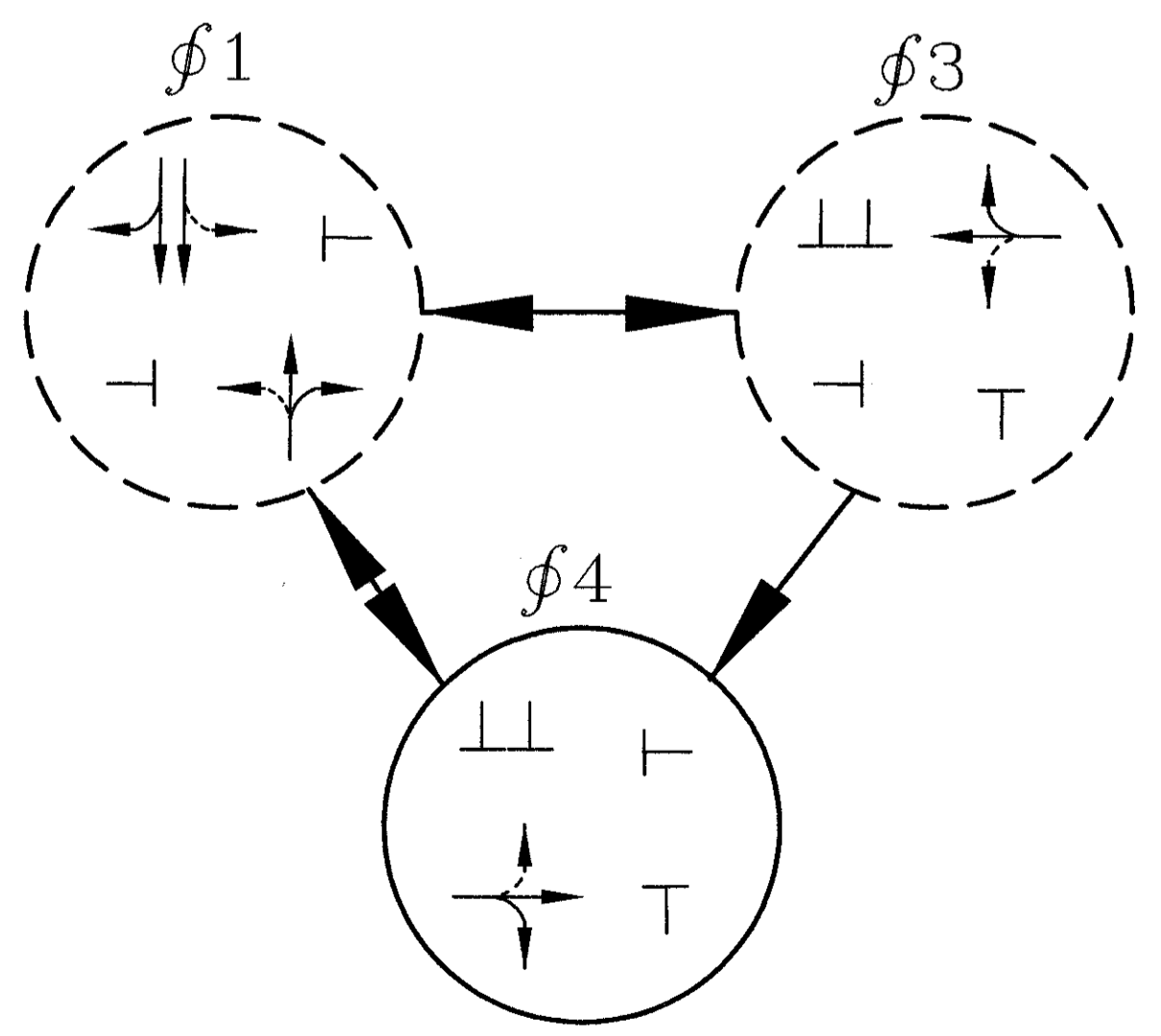


MATCH LINE STA. 8+00

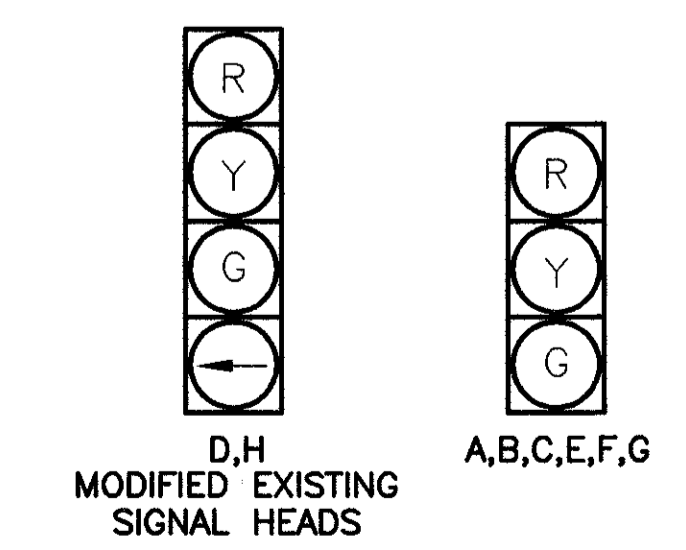
PROPOSED STORM SEWER TO BE CONSTRUCTION PRIOR TO INITIAL PHASE

TAPER TO BE USED 14 DAYS AFTER BEGINNING OF PHASE

TAPER TO BE USED 14 DAYS AFTER BEGINNING OF PHASE



EXISTING SIGNAL PHASING DIAGRAM



EXISTING SIGNAL HEADS

- ① TEMPORARY EDGE LINES
- ② TEMPORARY CENTER LINE
- ③ TEMPORARY STOP BAR
- ④ DOTTED LINE, WHITE
- ⑤ DOTTED LINE, YELLOW

LEGEND

- TRAFFIC FLOW
- ○ ○ ○ DRUMS SHALL BE SPACED 10' C/C
- PERMANENT PAVEMENT CONSTRUCTED THIS PHASE
- TEMPORARY PAVEMENT, CLASS B
- VEHICLE DETECTION ZONE
- PERMANENT PAVEMENT CONSTRUCTED THIS PHASE WITHIN FOURTEEN (14) DAYS