

ITEM 614 - MAINTAINING TRAFFIC

A MINIMUM OF ONE 10' LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, AND ITEM 615 TEMPORARY PAVEMENT.

THE CONTRACTOR SHALL NOTIFY THE DISTRICT OFFICE (216-581-2333, EXT. 244) EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK AND PRIOR TO CHANGING OF MAINTENANCE OF TRAFFIC PHASES.

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN THE AMOUNT OF \$1,250.00 PER DAY (IN ACCORDANCE WITH 108.07).

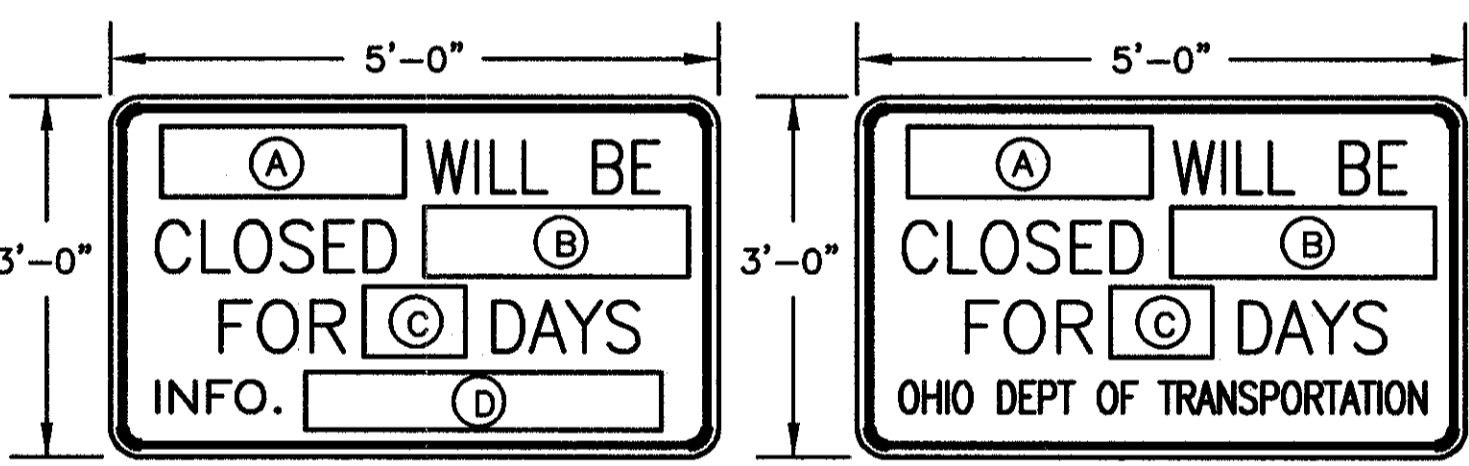
LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE WORK ZONES THAT ALTERNATELY CLOSE BOTH THE PASSING LANE AND THE DRIVING LANE UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES.

EXISTING SIGNS LOCATED WITHIN THE ROAD WORK AREAS WHICH ARE NECESSARY FOR INTERIM OR PERMANENT TRAFFIC CONTROL SHALL BE REMOVED AND REERECTED IN LOCATIONS AS APPROVED BY THE ENGINEER.

NOTICE OF CLOSURE SIGNS, AS DETAILED IN THESE PLANS, SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED ROAD CLOSURE. THE SIGNS SHALL BE ERECTED ON THE RIGHT HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.



- OC-60A OC-60B
- (A) RAMP, BRIDGE, ROAD (B) 3 LETTER MONTH - DOUBLE NUMBER, JUN 23
- (C) DOUBLE NUMBER, 60 (D) TELEPHONE NUMBER, 216-581-2333

QUANTITIES

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC.

410, TRAFFIC COMPACTED SURFACE, TYPE A OR B	1200 CU.YD.
614, BITUMINOUS CONCRETE FOR MAINTAINING TRAFFIC	100 CU.YD.
616, WATER	150 M GAL.
452, 6" PLAIN CONCRETE PAVEMENT, AS PER PLAN	105 SQ.YD.
452, 8" PLAIN CONCRETE PAVEMENT, AS PER PLAN	300 SQ.YD.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

TRENCH FOR WIDENING

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN BELOW 4 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILL AT THE DIRECTION OF THE ENGINEER.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR

IN ADDITION TO THE REQUIREMENTS OF 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER AND OFFICIAL PATROL CAR WITH WORKING TOP MOUNTED EMERGENCY FLASHING LIGHTS SHALL BE PROVIDED FOR CONTROLLING TRAFFIC AS DIRECTED BY ENGINEER FOR THE FOLLOWING TASKS:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED.
- DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

LAW ENFORCEMENT OFFICERS (L.E.O.'S) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED. THE LEO'S ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL PATROL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES WITH:

LAKE COUNTY SHERIFF DEPARTMENT
DANIEL A. DUNLAP
104 EAST ERIE STREET
PAINESVILLE, OH 44077
(440) 350-5528

LAW ENFORCEMENT OFFICERS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON AN HOURLY BASIS UNDER ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR 120 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

IF CONTRACTOR WISHES TO UTILIZE LEO'S FOR FLAGGING AND TRAFFIC CONTROL OTHER THAN FOR THAT REQUIRED IN THESE PLANS, HE MAY DO SO AT HIS OWN EXPENSE. PAYMENT FOR THE EXCESS ABOVE THE CONTRACT REQUIREMENTS WILL BE INCLUDED UNDER ITEM 614 MAINTAINING TRAFFIC.

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER AND CALCIUM CHLORIDE FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING CONTINGENCY QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

616, WATER	75 M. GAL.
616, CALCIUM CHLORIDE	7 TONS

ITEM SPECIAL - TEMPORARY TRAFFIC SIGNALS

THE CONTRACTOR SHALL CONSTRUCT TEMPORARY TRAFFIC SIGNALS AS SHOWN IN THE PLANS FOR THE FOLLOWING INTERSECTIONS:

- BACON/BOWHALL ROAD AND US ROUTE 20
- LANE ROAD AND US ROUTE 20
- CENTER ROAD AND US ROUTE 20

THE LOCATION OF THE SPAN WIRE MOUNTED SIGNAL HEADS SHALL BE ADJUSTED AS REQUIRED FOR EACH CONSTRUCTION PHASE TO PROVIDE PROPER ALIGNMENT WITH THE APPROACH TRAFFIC LANES. CONTROLLER TIMING SETTINGS SHALL BE ADJUSTED AS NECESSARY TO OPTIMIZE TRAFFIC FLOW FOR EACH CONSTRUCTION PHASE.

TEMPORARY SIGNAL SYSTEMS SHALL CONSIST OF 35 FEET MINIMUM LENGTH, WOOD POLES CLASS I, GUYS FOR ALL STRAIN POLES, MESSENGER WIRE AND ALL OTHER EQUIPMENT NECESSARY TO INSTALL AND OPERATE THE SIGNAL SYSTEMS IN ACCORDANCE WITH ODOT SPECIFICATION 614.03 (F) AND THE PLANS.

SIGNALS SHALL BE INSTALLED AND OPERATED IN ACCORDANCE WITH THE REQUIREMENTS OF PART 6 OF THE OHIO MANUAL OF UNIFORM TRAFFIC DEVICES.

SIGNAL WIRING OTHER THAN OVERHEAD AND UNDERGROUND MUST BE APPROVED BY THE ENGINEER. ALL WIRING ATTACHED TO A POLE SHALL BE PROTECTED BY THE APPROPRIATE SIZE CONDUIT. ALL SPLICES SHALL BE MADE BY APPROVED SPLICE KITS INSTALLED IN PULL BOXES OR APPROVED ENCLOSURES.

THE CONTROLLER TO BE USED SHALL BE AN ACTUATED SOLID STATE DIGITAL MICROPROCESSOR WITH POLE MOUNTED CABINET IN CONFORMANCE WITH THE C & M SPECIFICATIONS. THE CONTROLLER SHALL BE CAPABLE OF HANDLING THE OPERATION SHOWN ON THE PHASING AND TIMING DIAGRAM.

THE TEMPORARY DETECTOR UNIT SHALL BE: (1) A MAGNETIC SENSOR ATTACHED TO A LIGHT WOOD POST AT ROADSIDE, (2) LOOPS OR MAGNETOMETERS FASTENED TO THE DRIVE SURFACE OR MICROWAVE DETECTORS, MOUNTED ON A SUITABLE SUPPORT BESIDE OR OVER THE ROAD. THE DETECTOR UNIT LOCATION SHALL BE APPROVED BY THE ENGINEER.

ALL SIGNAL EQUIPMENT SHALL CONFORM TO ITEM 632 EXCEPT USED EQUIPMENT MAY BE USED AND THE 10 DAY PERFORMANCE TEST IS WAIVED.

PAYMENT FOR THE TEMPORARY SIGNALS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM SPECIAL - TEMPORARY TRAFFIC SIGNAL AND SHALL INCLUDE ALL EQUIPMENT, MATERIALS, AND LABOR REQUIRED TO CONSTRUCT, MAINTAIN, AND SUBSEQUENTLY REMOVE THE TEMPORARY SIGNALS FROM SERVICE.

EXISTING SIGNAL MODIFICATIONS AT TOWNLIN ROAD

THE EXISTING SIGNAL AT TOWNLIN ROAD SHALL BE REVISED BY THE CONTRACTOR TO HANDLE MAINTENANCE OF TRAFFIC PLANS TO INCLUDE THE ADJUSTING OF SIGNAL HEADS TO ALIGN WITH THE APPROACH LANE(S), PROVIDING TEMPORARY DETECTORS, SIGNAL TIMING, AND PHASING. PAYMENT FOR THIS WORK SHALL BE INCLUDED UNDER ITEM 614 - MAINTAINING TRAFFIC

STORM SEWER WORK AT FAIRPORT NURSERY ROAD

THE STORM SEWER WORK AT FAIRPORT NURSERY ROAD WILL NEED TO BE DONE AT NIGHT (10:00P.M. TO 5:00A.M.) BY THE CONTRACTOR BY USING PARTIAL WIDTH CONSTRUCTION WITHIN A WORK ZONE CONTROLLED BY AT LEAST THREE FLAGGERS AND FLOOD LIGHTS. STANDARD DRAWING MT-97.10 SHALL BE USED AS A REFERENCE TO ADAPT THE MAINTENANCE OF TRAFFIC TO THE WORK AT THE INTERSECTION. IF NEEDED AND APPROVED BY THE ENGINEER THE RIGHT LANE OF U.S.R. 20 WESTBOUND MAY BE CLOSED PER STANDARD DRAWING MT-95.31 AND THE LEFT LANE OF U.S.R. 20 EASTBOUND PER MT-95.32. THESE LANE CLOSURES ON U.S.R 20 SHALL BE UTILIZED TO DEVELOP RIGHT AND LEFT TURN LANES THAT CAN BE BETTER CONTROLLED BY FLAGGERS.

ITEM SPECIAL - REPLACEMENT SIGN

FLAT SHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL TO THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED BUT GOOD CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER SQUARE FEET FOR ITEM SPECIAL, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 100 SQUARE FEET HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

TEMPORARY WORK ZONE SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR REPLACEMENT OF TEMPORARY WORK ZONE SIGNS DAMAGED DURING CONSTRUCTION.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - WORK ZONE MARKING SIGN 20 EACH

ITEM SPECIAL - REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM SPECIAL, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 50 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614 - WORK ZONE SPEED LIMIT SIGN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN, COVER DURING SUSPENSION OF WORK, AND SUBSEQUENTLY REMOVE WORK ZONE SPEED LIMIT SIGNS AND SUPPORTS (R-10) WITHIN THE WORK LIMITS IN ACCORDANCE WITH THE FOLLOWING REQUIREMENTS.

THE CONTRACTOR SHALL COVER OR REMOVE ANY EXISTING SPEED LIMIT OR MINIMUM SPEED SIGNS WITHIN THE REDUCED SPEED ZONE. THESE SIGNS SHALL BE RESTORED DURING SUSPENSION OR TERMINATION OF THE REDUCED SPEED LIMIT. THE EXPENSE OF COVERING OR REMOVAL AND RESTORATION OF EXISTING SPEED LIMIT OR MINIMUM SPEED SIGNS SHALL BE INCLUDED IN THE PAY ITEM FOR THE WORK ZONE SPEED LIMIT SIGNS.

THE WORK ZONE SPEED LIMIT SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN 4 HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN 4 HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER.

A SIGN(S) TO INDICATE THE RESUMPTION OF THE STATUTORY SPEED LIMIT SHALL BE ERECTED AT THE END OF ANY REDUCED SPEED ZONE. R-10 SIGNS (SPEED LIMIT) SHALL BE USED ON UNDIVIDED ROADWAY. R-10 (SPEED LIMIT) AND R-9A SIGNS (SPEED LIMIT) SHALL BE USED ON DIVIDED ROADWAYS. WHEN USED THE R-10 AND R-9A SIGNS SHALL BE MOUNTED SIDE-BY-SIDE ON SEPARATE SUPPORTS.

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED BUT GOOD CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE REFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF 730.19 AND U.S. DEPARTMENT OF TRANSPORTATION SUPPLEMENTAL SPECIFICATION FOR TYPE III-C SHEETING, FP-85. WORK ZONE SPEED LIMIT SIGNS SHALL BE MOUNTED ON TWO (2) ITEM 630 GROUND MOUNTED SUPPORTS, NO. 3 POSTS.

WORK ZONE SPEED LIMIT SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGNS AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION WITHIN THE PROJECT DUE TO CHANGES IN THE SPEED ZONE DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.