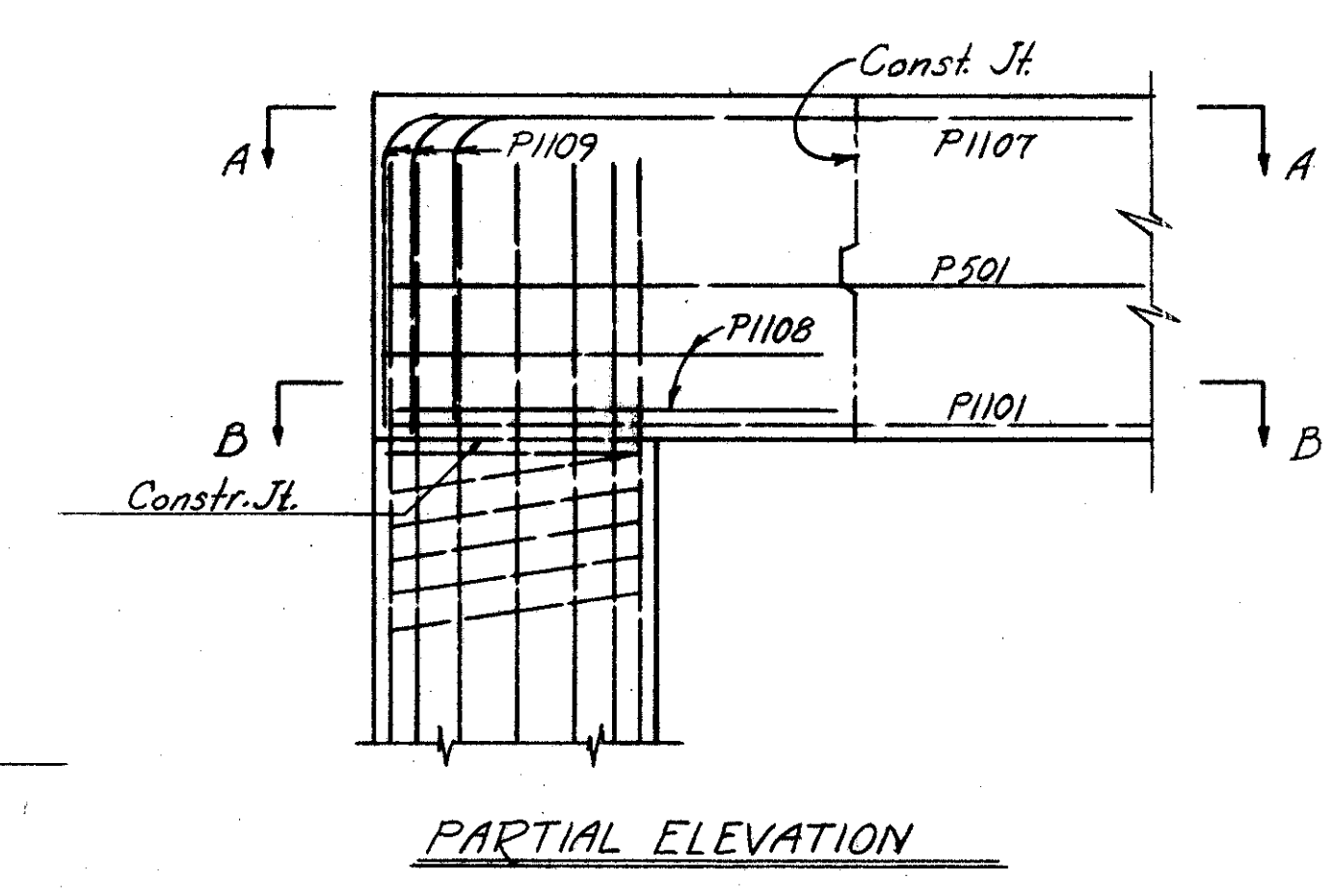
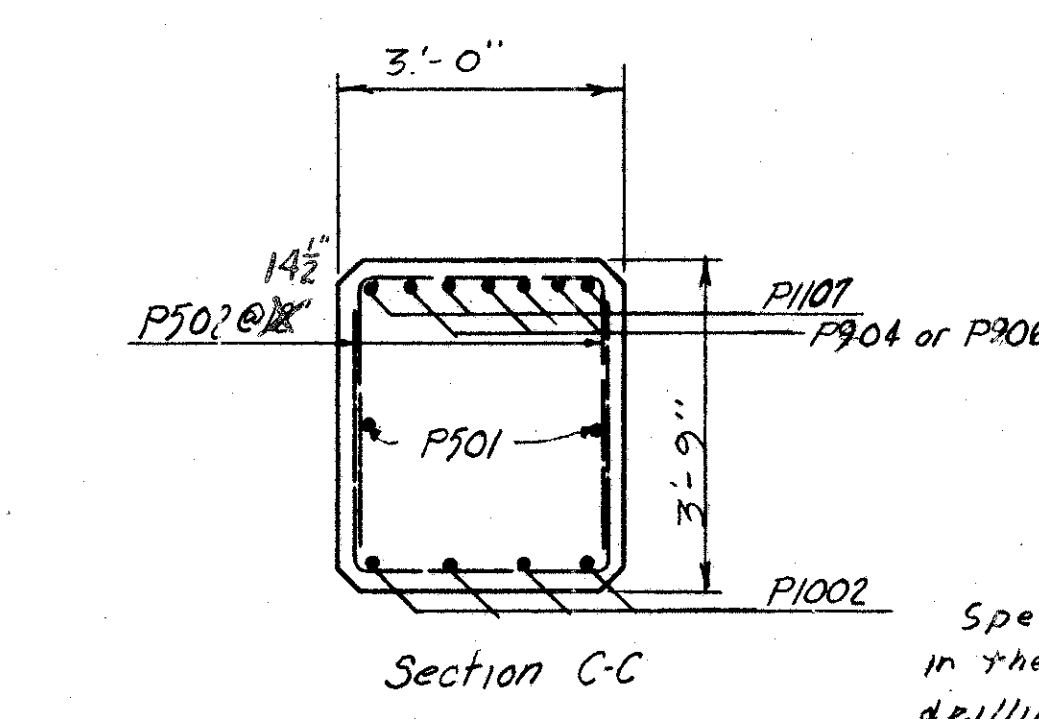
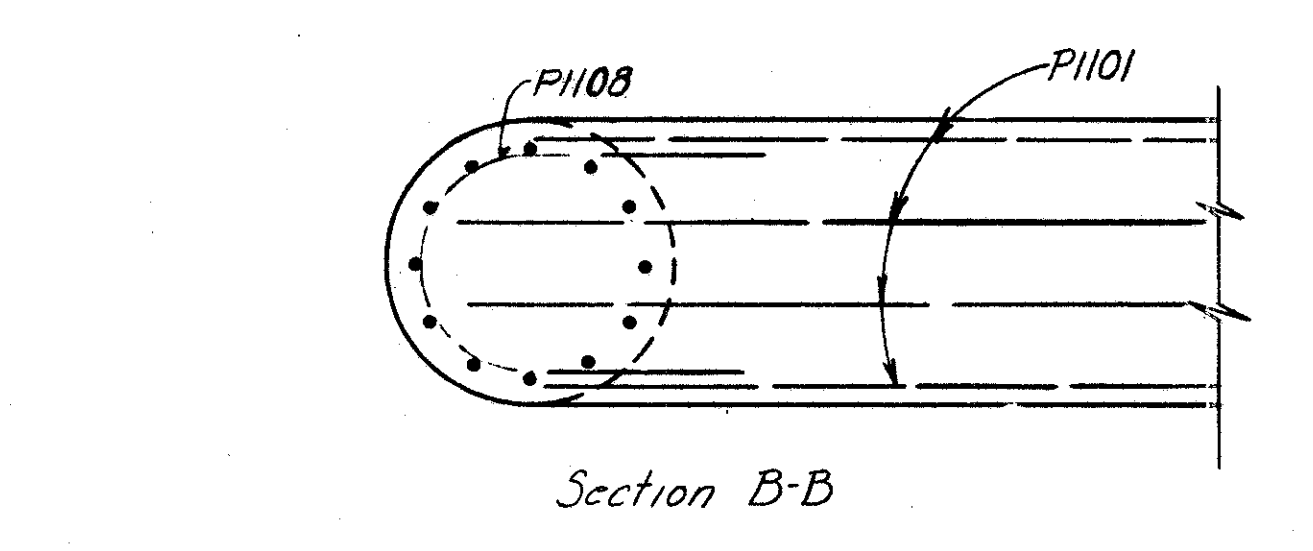
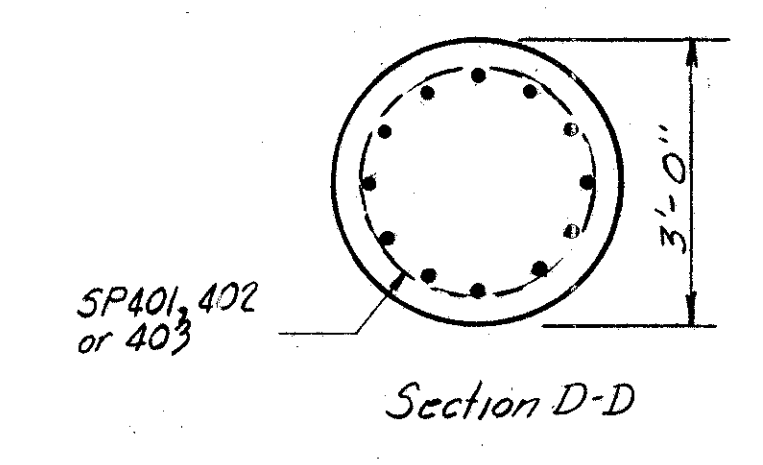
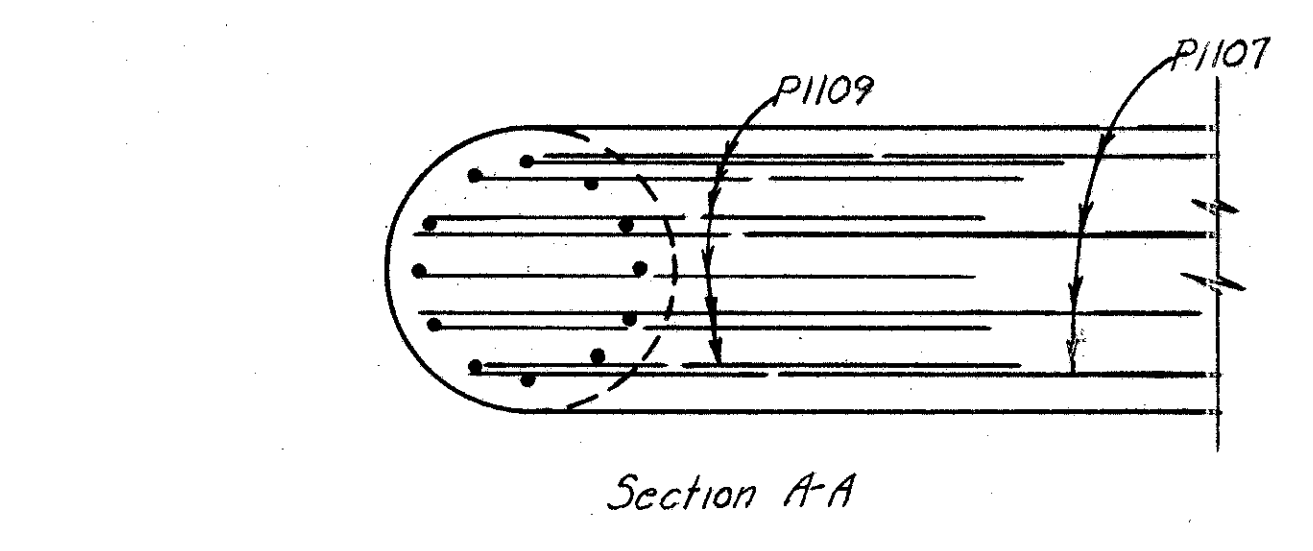
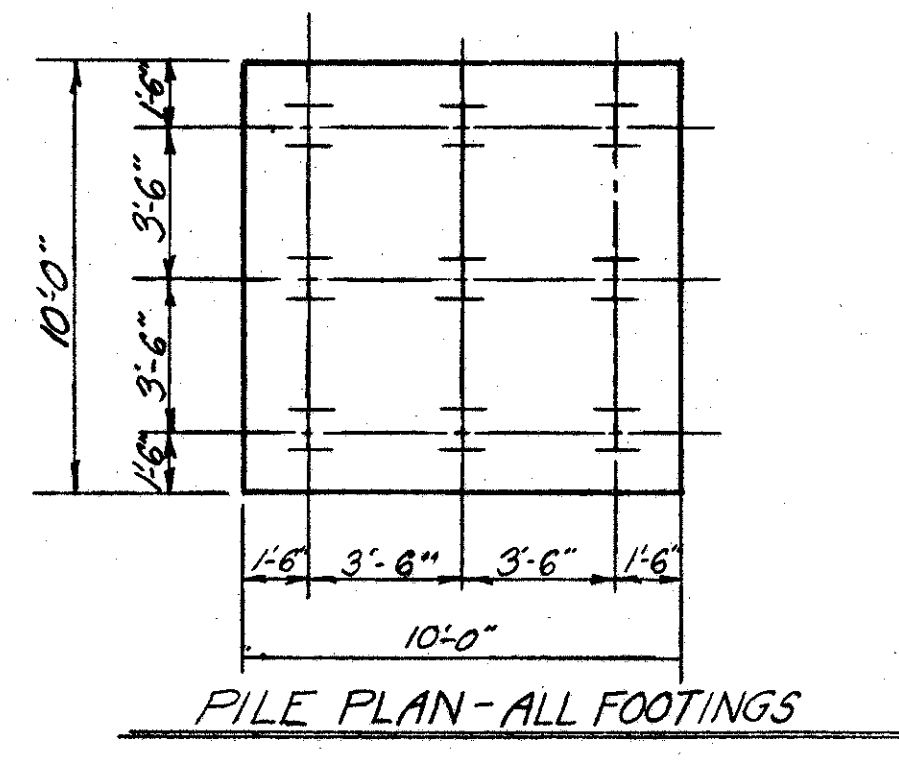
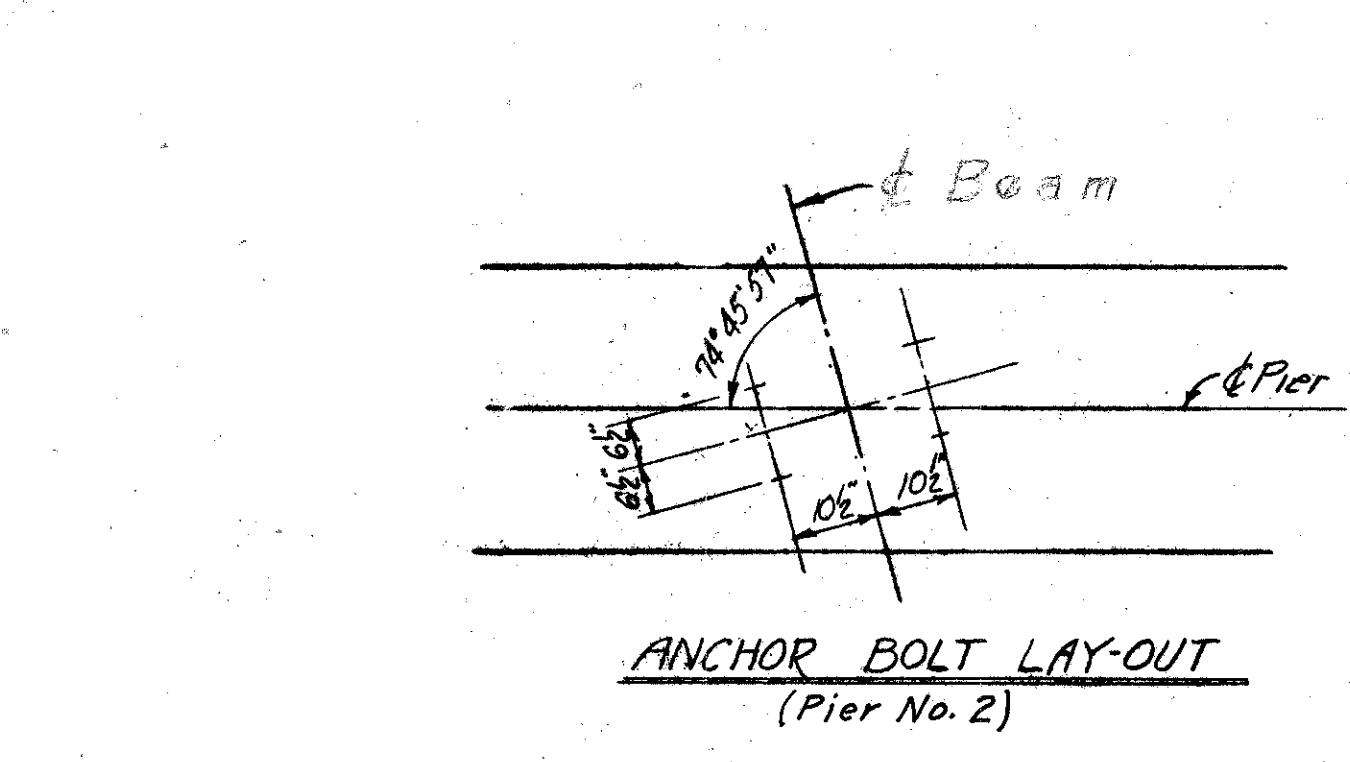
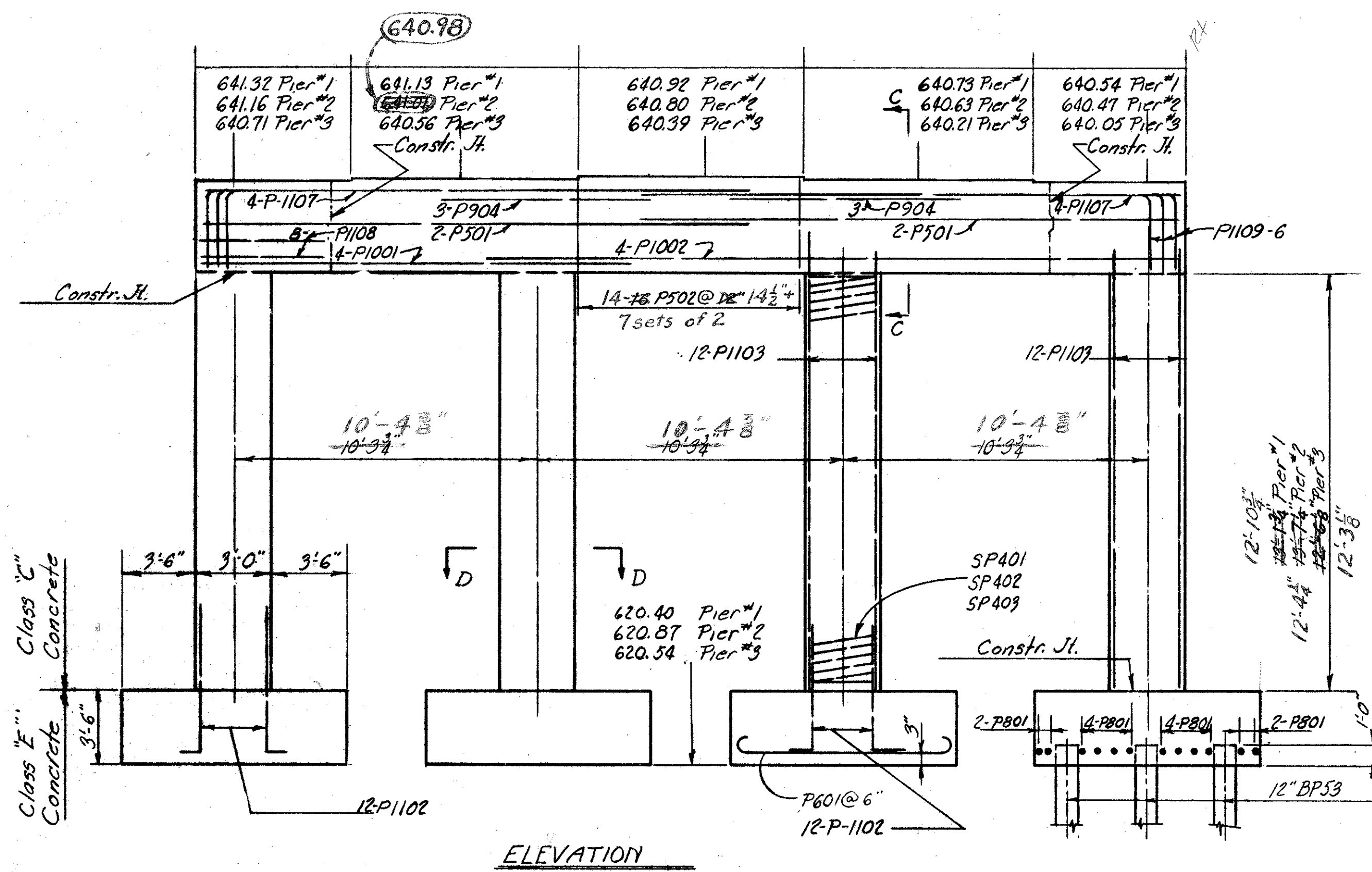
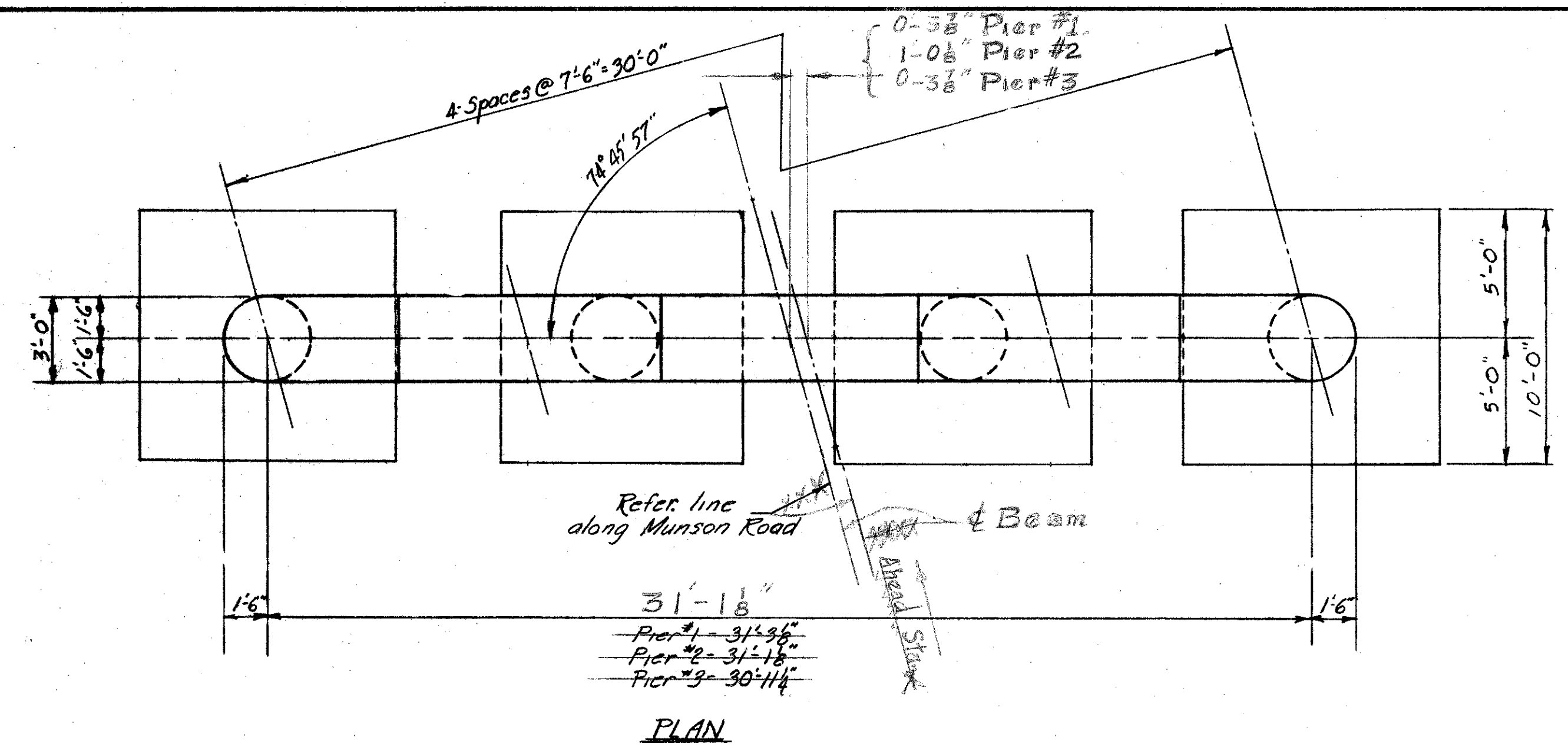


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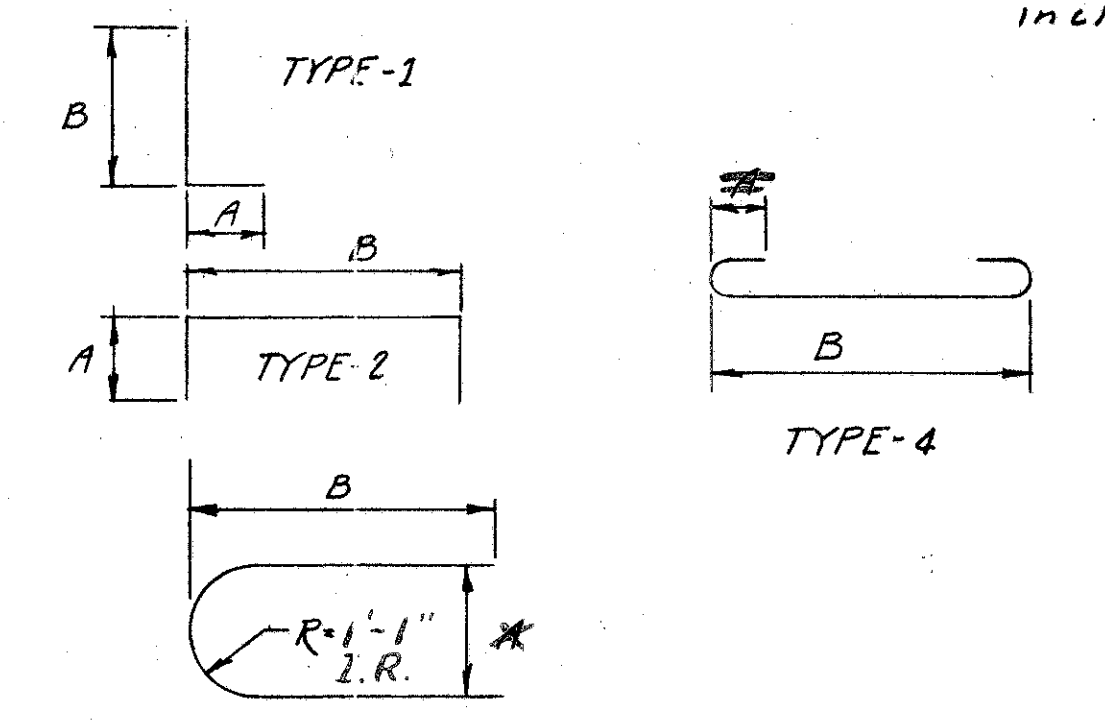
PIER NOTES

Special Care shall be taken in placing reinforcing steel in the pier cap so that it will not interfere with the drilling of anchor bolt holes.
Reinforcing steel shall clear the face of concrete by 2" unless otherwise noted.
All pier details and reinforcement are symmetrical about the pier, unless otherwise noted.
Piles shall have a maximum capacity of 41 Tons per pile

SPIRAL NOTES

The length shown in the bar list for spiral bars is the distance from the top of the footing to the bottom of the cap.
The no. of turns shown is the length divided by the pitch plus 3 turns (total no. closed coils) expressed to the nearest whole no.
Spiral bars shall have no deformations, but shall in other respects conform to item 5-4
1/2 closed coils shall be provided at the ends of each unit
Four steel channel, or an angle spec., weighing approx. 0.68 lbs per lin ft, shall be provided for each spiral unit.
They shall be equally spaced along the periphery of the coil. The number of pounds of spacers based on 0.68 lbs per lin ft. will be paid for as reinforcing steel and is included in the tabulated quantities of spiral bars

BAR SCHEDULE						
MARK	NO.	LENGTH	TYPE	A	B	WEIGHT
P1001	12	12'-3"	SH			635
P1002	12	24'-0"	SH			1265
P1103	144	15'-9"	SH			10700
P902	18	10'-3"	SH			825
P501	12	19'-0"	SH			238
P1107	24	19'-0"	SH			2420
P1108	12	10'-9"	SH	4'-9"		685
P1109	36	9'-8"	1	3'-2"	6'-6"	1840
P502	4	9'-0"	2	3'-2"	2'-8"	1183
P801	144	10'-10"	4	9'-6"		2342
P801	144	11'-8"	4	9'-6"		4487
P1102	144	7'-11"	1	1'-0"	6'-11"	6062
						36,752



Mark	No.	Size	Length	Pitch	No. Turns	Core Dia.	Weight
SP401	4	1/2"	13'-0"	4 1/2"	38	32"	992
SP402	4	1/2"	12'-4"	4 1/2"	37	32"	964
SP403	4	1/2"	12'-4"	4 1/2"	37	32"	964
							3240
							Total

Revised 1-21-59
Revised 2-16-59
SEC. L-32 FED. AID PROJ. NO. F-329(16)
PREPARED BY
CAPITOL ENGINEERING ASSOCIATES, DILLSBURG, PA.
FOR
STATE OF OHIO
DEPARTMENT OF HIGHWAYS
DIVISION OF DESIGN AND CONSTRUCTION
BUREAU OF BRIDGES
PIER DETAILS
BRIDGE NO. LAK-2-1025
RELOC. SR2 UNDER MUNSON ROAD
LAKE COUNTY
STA. 441+28.08

DESIGNED	DRAWN	TRACED	CHECKED	REVISED DATE	REVISION
M			RJP	4-24-59	4-1-59