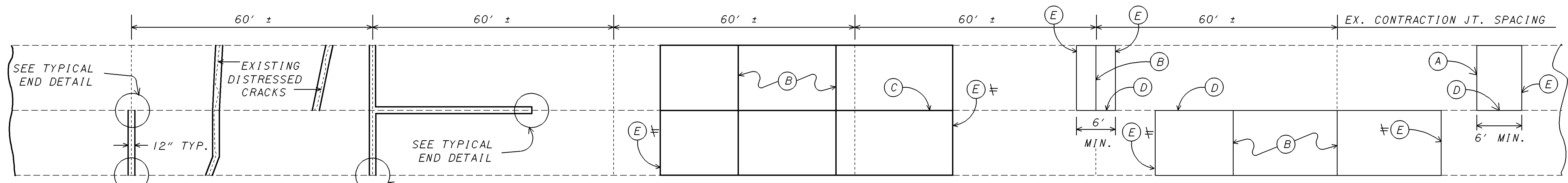


← DIRECTION OF TRAFFIC →

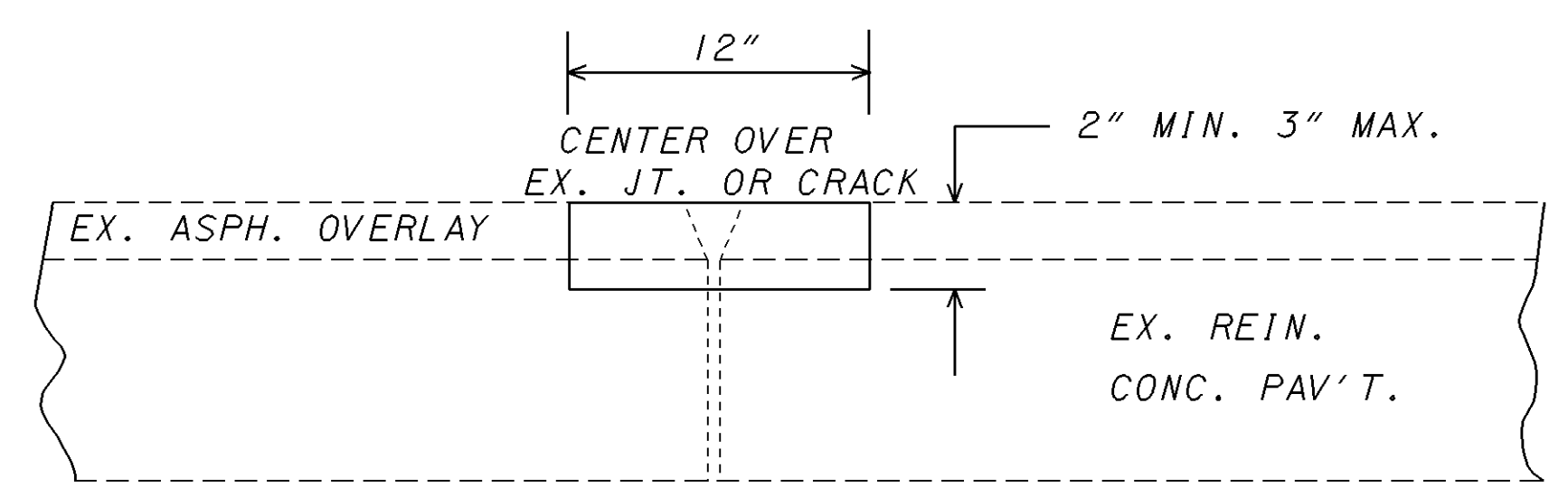


PARTIAL DEPTH JOINT OR CRACK REPAIR

TYPICAL TWO LANE REPLACEMENT

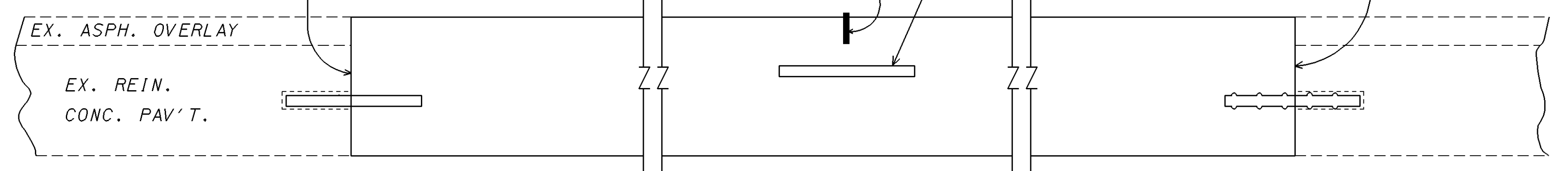
TYPICAL ONE LANE REPLACEMENT

≠ USE (A) JOINT IF EITHER ADJACENT (EXISTING OR PROPOSED) CONTRACTION JOINT IS FARTHER THAN 20 FEET.



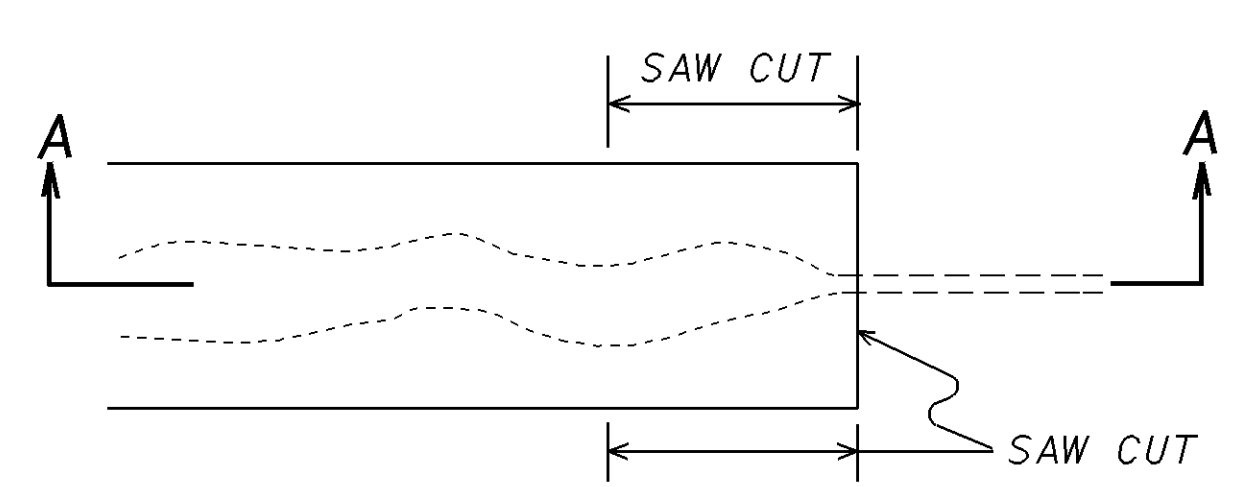
ITEM 251 - PARTIAL DEPTH PAV'T REPAIR

- CRACK REPAIRS (NO JOINT WITHIN REPAIR) - (A)
- JOINT REPAIRS (LESS THAN 10 FEET) - (E)
- PANEL REPAIRS (GREATER THAN 10 FEET) - (E) ≠

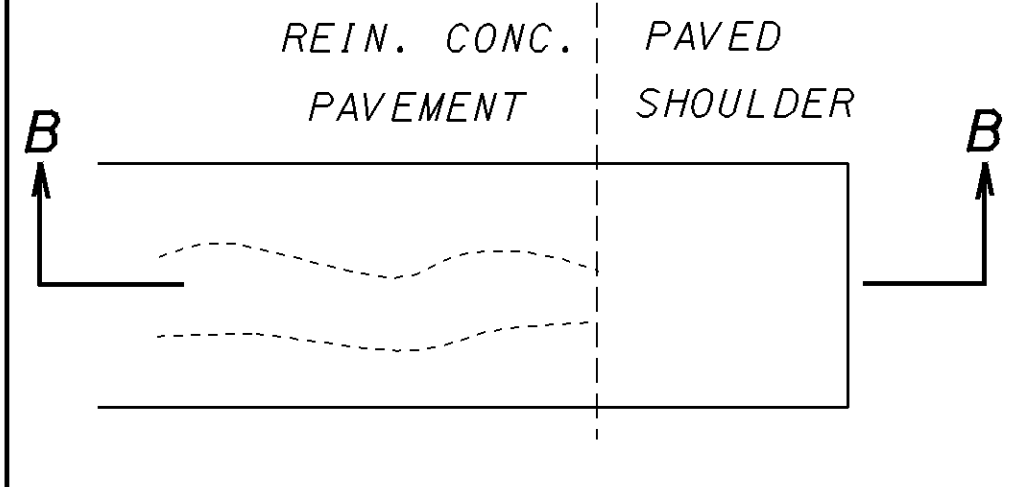


ITEM 255 - FULL DEPTH RIGID PAVEMENT REMOVAL AND RIGID REPLACEMENT

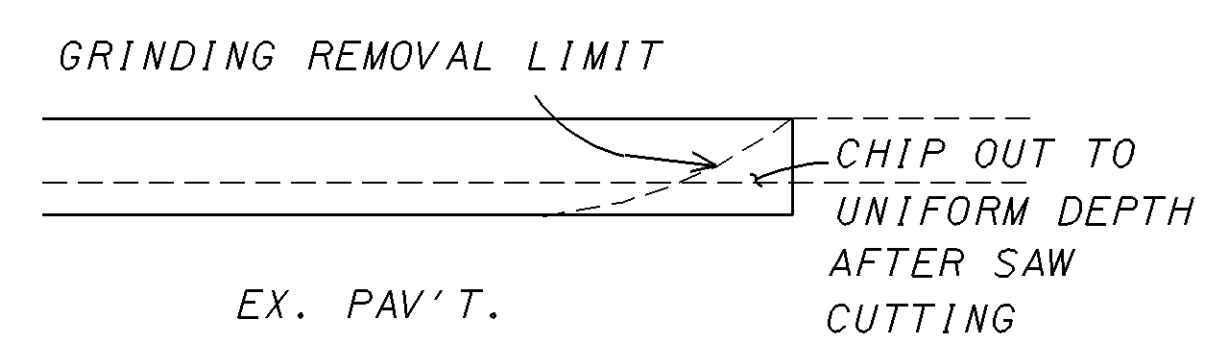
SEE GENERAL NOTES ON SHEET 15 FOR ADDITIONAL INFORMATION.



DISTRESSED JOINT-PLAN VIEW

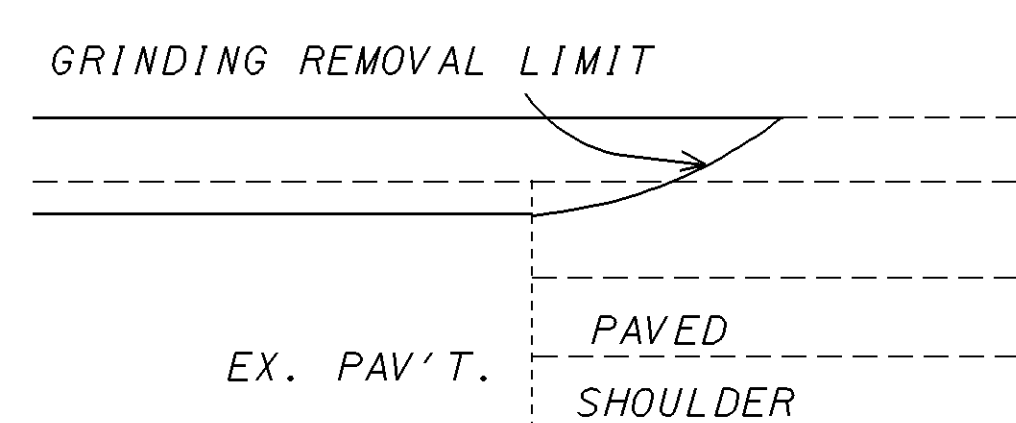


SHOULDER TREATMENT DETAIL



TYPICAL END DETAIL

NO SEPARATE PAYMENT WILL BE MADE FOR THESE SAW CUTS



SHOULDER TREATMENT DETAIL

MEASURED QUANTITY SHALL NOT INCLUDE THE PAVED SHOULDER AREA

SEE GENERAL NOTES ON SHEET NO. 15 FOR ADDITIONAL INFORMATION.

LEGEND

- (A) TYPE Y DOWELLED REPAIR JOINTS, AS PER BP-2.5
- (B) SAWED CONTRACTION JOINT AS PER BP-2.2, WITH DOWELS, MAX. SPACING 20' C/C FOR ONE LANE REPLACEMENTS ALIGN JOINT WITH EXISTING CRACKS IN THE ADJACENT LANE WHENEVER POSSIBLE. (EX. CRACKS OCCUR APPROX. 15' C/C)
- (C) LONGITUDINAL BUTT JOINT AS PER BP-2.1 (USING HOOK BOLTS)
- (D) TYPE D JOINT AS PER BP-2.1 FOR PATCHES 10' OR GREATER IN LENGTH
- (E) TYPE T TIED REPAIR JOINT, AS PER BP-2.5

ESTIMATED QUANTITIES*		
ITEM 255	FULL DEPTH RIGID PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS FS, AS PER PLAN A	5000 SQ.YD.
ITEM 255	FULL DEPTH RIGID PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS FS, AS PER PLAN B	2100 SQ.YD.
ITEM 255	FULL DEPTH PAVEMENT SAWING	20,554 LIN. FT.
ITEM 203	EXCAVATION NOT INCLUDING EMBANKMENT CONSTRUCTION	50 CU. YD.
ITEM 304	AGGREGATE BASE, AS PER PLAN	50 CU. YD.

ESTIMATED QUANTITY *

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN 10,000 SQ. YD.

* QUANTITY ESTIMATES ARE BASED ON VISUAL INSPECTION. AN ADDITIONAL 5% WAS ADDED TO THE REPAIR AREA TO COMPENSATE FOR ANY ROADWAY DETERIORATION THAT MAY OCCUR BETWEEN THE TIME OF PLAN PREPARATION AND ACTUAL CONSTRUCTION.

VISUAL SURVEY DATE - 7/24/01

SR- 2, EXISTING 10" REINFORCED PORTLAND CEMENT CONCRETE SURFACE AREA - 170,015 S. Y.
SR- 2, EXISTING 11" REINFORCED PORTLAND CEMENT CONCRETE SURFACE AREA - 5070 S. Y.

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