

TABLE A  
DISTANCE FOR DETERMINING SEAL GLAND SIZE ①

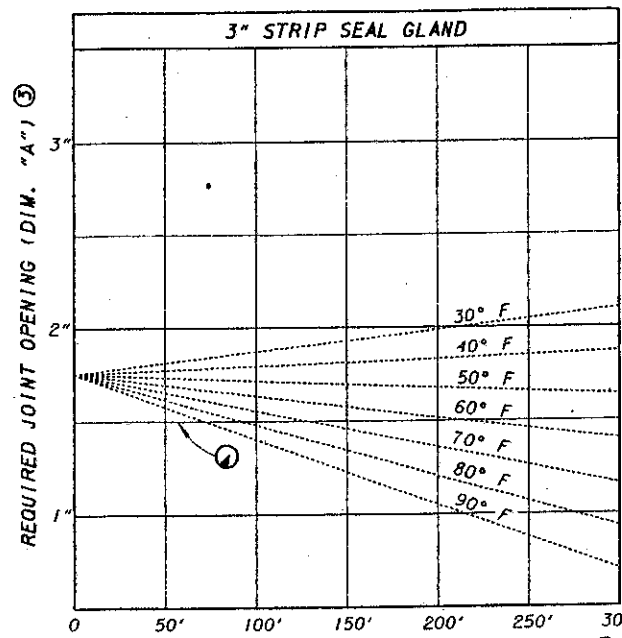


TABLE B  
DISTANCE FOR DETERMINING JOINT OPENING ②

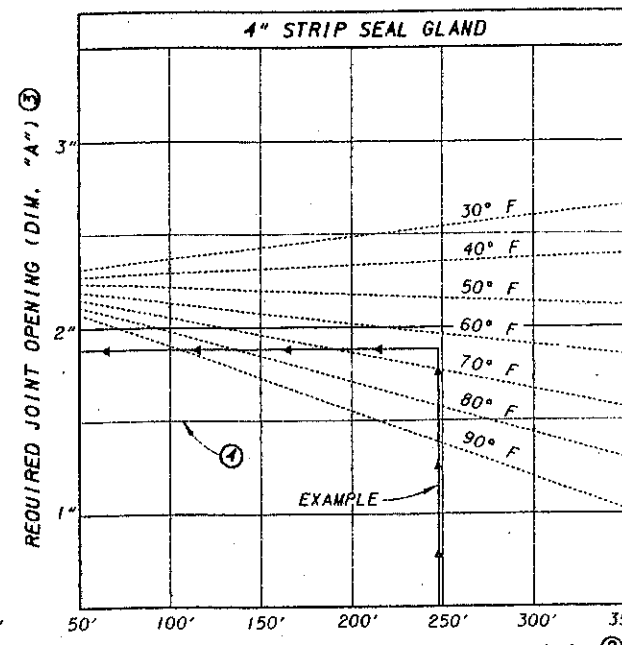


TABLE C  
DISTANCE FOR DETERMINING JOINT OPENING ②

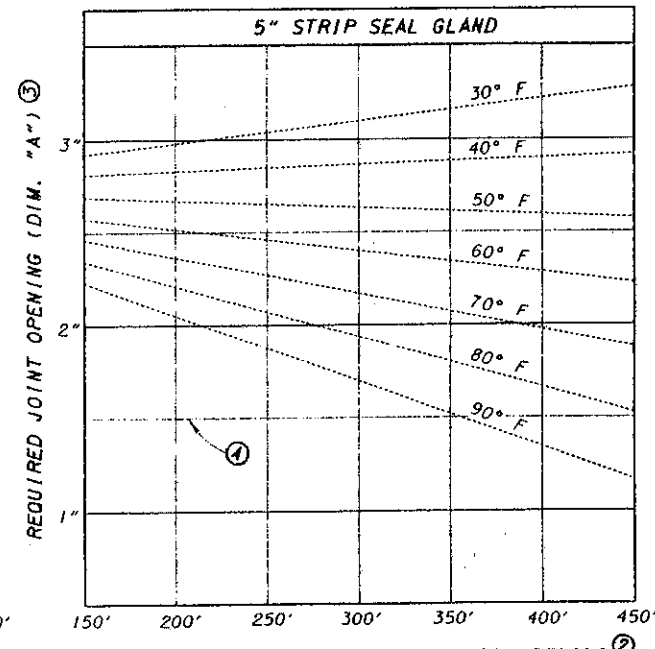


TABLE D  
DISTANCE FOR DETERMINING JOINT OPENING ②

**GENERAL NOTES:**

**MATERIALS:** ALL STEEL PARTS OF THE JOINT ASSEMBLY SHALL BE ASTM A709, GRADE 36. THE FINISHED STEEL ASSEMBLY SHALL BE METALIZED. THE THICKNESS OF THE COATING SHALL BE 6-8 MILS. THE WIRE USED FOR THE METALLIZING SHALL CONSIST OF 85% ZINC AND 15% ALUMINUM. SURFACE PREPARATION AND APPLICATION SHALL CONFORM TO SSPC COATING SYSTEM GUIDE NO. 2300, "GUIDE FOR THERMAL SPRAY METALLIC COATING SYSTEMS". A SEALER AS PER SSPC PAINT SPECIFICATION NO. 27, SHALL BE APPLIED TO METALLIZED SURFACES THAT WILL BE IN CONTACT WITH CONCRETE. THERE IS NO NEED TO METALIZE THE SURFACES OF THE RETAINERS TO WHICH THE GLANDS WILL BE BONDED. THESE SURFACES MAY BE MASKED TO PREVENT THE BUILDUP OF OVER-SPRAY WITHIN THE RETAINER GROOVE.

THE PREFORMED STRIP SEAL GLAND SHALL BE EXTRUDED POLYCHLOROPRENE MATERIAL MEETING THE REQUIREMENTS OF ASTM D2628. DUE TO THE CONFIGURATION OF THE STRIP SEAL, THE RECOVERY TEST IS NOT APPLICABLE. PHYSICAL PROPERTIES SHALL MEET THE REQUIREMENTS SPECIFIED IN TABLE "E" ON THIS SHEET.

EACH LOT OF STRIP SEAL GLANDS SHALL BE TESTED BY THE MANUFACTURER OR AN ACCREDITED LABORATORY TO ENSURE COMPLIANCE WITH THESE PROVISIONS. TWO CERTIFIED COPIES OF THE QUALIFICATION TEST DATA INDICATING THAT THE TESTED MATERIALS COMPLY WITH THESE PROVISIONS SHALL BE SUBMITTED TO THE ODOT TESTING LABORATORY.

THE STEEL RETAINER AND POLYCHLOROPRENE GLAND SHALL BE SUPPLIED BY THE SAME MANUFACTURER AND SHALL BE DESIGNED TO FUNCTION AS AN INTEGRAL UNIT.

LUBRICANT-ADHESIVE USED TO INSTALL THE PREFORMED STRIP SEALS SHALL BE A ONE PART MOISTURE CURING POLYURETHANE COMPOUND, MEETING THE REQUIREMENTS OF ASTM D4070, AND AS SPECIFIED BY THE SEAL GLAND MANUFACTURER. IT SHALL HAVE A SUITABLE CONSISTENCY AT THE TEMPERATURE AT WHICH THE SEALS ARE INSTALLED AND SHALL BE COMPATIBLE WITH THE SEALS AND THE STEEL RETAINERS.

**SPLICE OR JOINT IN SEAL GLAND:** SEAL GLANDS FOR BRIDGE DECK JOINTS SHALL BE FURNISHED IN ONE CONTINUOUS PIECE UNLESS SHOP FABRICATED SPLICES ARE SHOWN ON THE PLANS OR APPROVED BY THE DIRECTOR. FIELD SPLICING SHALL NOT BE PERMITTED.

COMPLETED SPLICES SHALL HAVE NO OFFSETS ON EXTERIOR SURFACES, AND AFTER INSTALLATION, THERE SHALL BE NO EVIDENCE OF BOND FAILURE AT THE SPLICES.

FOR OTHER THAN STRAIGHT SEALS WITHOUT INTERMEDIATE SPLICES, SEAL GLANDS SHALL BE SHOP FABRICATED IN ACCORDANCE WITH APPROVED SHOP DRAWINGS, SHOP DRAWING DIMENSIONS FOR EXISTING JOINTS OR FOR JOINTS WHICH ARE BEING MODIFIED SHALL BE BASED ON FIELD MEASUREMENTS PROVIDED BY THE CONTRACTOR.

**PREPARATION FOR INSTALLATION:** TO AVOID THE SUBSEQUENT CONTAMINATION OF THE PREPARED SURFACES, ALL SURFACES OF ELASTOMERIC STRIP SEAL GLANDS SHALL BE CLEANED WITH METHYL ETHYL KETONE (MEK), TOLUENE (T) OR ANOTHER APPROVED SOLVENT USING CLEAN DISPOSABLE CLOTHS.

NO MORE THAN 24 HOURS BEFORE APPLICATION OF THE LUBRICANT ADHESIVE, ONLY THE SURFACES OF THE STEEL RETAINER TO WHICH ADHESIVE IS APPLIED SHALL BE CLEANED TO SSPC VISUAL STANDARD SP-6.

**INSTALLATION:** IMMEDIATELY PRIOR TO APPLICATION OF LUBRICANT-ADHESIVE, BONDING SURFACES SHALL BE CLEAN, DRY AND WARMER THAN 45 DEGREES F. AND THEY SHALL BE MAINTAINED AT OR ABOVE THIS TEMPERATURE UNTIL THE ADHESIVE HAS CURED. LUBRICANT-ADHESIVE SHALL BE APPLIED LIBERALLY TO BOTH STEEL AND ELASTOMERIC BONDING SURFACES USING A STIFF BRUSH IF NECESSARY TO ACHIEVE A COMPLETE AND RELATIVELY UNIFORM COATING. THE BULBED EDGES OF THE ELASTOMERIC SEAL SHALL BE INSERTED INTO THE RETAINER GROOVES. AFTER INSTALLATION, EXCESS LUBRICANT-ADHESIVE SHALL BE REMOVED FROM THE EXPOSED SEAL SURFACES.

SEAL GLANDS SHALL BE INSTALLED WITH EQUIPMENT DESIGNED OR SPECIFICALLY ADAPTED FOR THE INSTALLATION OF ELASTOMERIC JOINT SEAL GLANDS. THIS EQUIPMENT SHALL NOT ELONGATE THE SEAL GLAND OR CAUSE STRUCTURAL DAMAGE TO THE COMPLETED INSTALLATION.

REPAIRS SHALL BE MADE PRIOR TO THE INSTALLATION OF THE SEAL. METALLIZED SURFACES DAMAGED DURING FABRICATION SHALL BE REPAIRED BY REBLASTING AND METALLIZING AS PER SSPC GUIDE 23.00. METALLIZED SURFACES DAMAGED DURING SHIPMENT OR FIELD WELDING SHALL BE REPAIRED AS PER ASTM A 780-93a, ANNEX A1, REPAIR USING ZINC BASED ALLOYS. THIS FIELD PROCESS REQUIRES REMOVAL OF CONTAMINATES FROM THE SURFACE, PREHEATING THE SURFACE TO 600° F AND APPLICATION OF ZINC COATING BY EITHER RUBBING A PURE ZINC STICK OR SPRINKLING ZINC POWDER ON THE PREHEATED SURFACE. THE ZINC COATING THICKNESS SHALL BE THE SAME AS THAT SPECIFIED FOR THE METALLIZING.

MEASUREMENT FOR PAY PURPOSES SHALL BE BASED ON THE LINEAR FEET OF SEALED JOINT SYSTEM, MEASURED HORIZONTALLY ALONG THE JOINT CENTERLINE AND BETWEEN THE OUTER LIMITS OF THE FABRICATED JOINT. THIS PAY ITEM SHALL INCLUDE ALL LABOR, MATERIALS AND EQUIPMENT NECESSARY TO COMPLETE THE JOINT IN PLACE, WHICH INCLUDES: THE JOINT ARMOR, RETAINERS, GLAND, ANCHORING DEVICES, PLATE "A", PLATE "B", TEMPORARY SUPPORTS AND THE END CROSSFRAME TOP GUSSET PLATES. PAYMENT WILL BE MADE PER LINEAR METER FOR ITEM 516, "STRUCTURAL EXPANSION JOINTS, INCLUDING ELASTOMERIC STRIP SEALS".

THERMAL NEUTRAL POINT OF THE SUPERSTRUCTURE IS THAT POINT WHICH HAS ZERO HORIZONTAL MOVEMENT DURING TEMPERATURE CHANGES.

**LEGEND**

- ① - THIS IS THE ACTUAL DISTANCE FROM THE CENTERLINE OF JOINT TO THE THERMAL NEUTRAL POINT OF THE SUPERSTRUCTURE MEASURED ALONG THE CENTERLINE OF ROADWAY. THIS DIMENSION SHALL BE A MAXIMUM OF 299' FOR 60° SKEWS, 342' FOR 45° SKEWS, 385' FOR 30° SKEWS AND 427' FOR 0° THRU 15° SKEWS.
- ② - THIS DISTANCE FOR EXPANSION JOINTS HAVING SKEW ANGLES OF 15° OR LESS IS THE ACTUAL DISTANCE TO THE THERMAL NEUTRAL POINT OF THE SUPERSTRUCTURE ALONG CENTERLINE OF ROADWAY. THIS DISTANCE FOR EXPANSION JOINTS HAVING SKEW ANGLES OVER 15° BUT NOT GREATER THAN 60° IS ARRIVED AT BY MULTIPLYING THE ABOVE DEFINED DISTANCE ALONG THE CENTERLINE OF ROADWAY BY THE COSINE OF THE EXPANSION JOINT SKEW ANGLE.
- ③ - THIS IS THE JOINT OPENING (DIMENSION "A") REQUIRED AT THE TIME OF ABUTMENT BACKWALL CONCRETE PLACEMENT, BASED ON THE DAY'S ANTICIPATED PEAK AMBIENT TEMPERATURE.
- ④ - MINIMUM JOINT OPENING (DIMENSION "A") AT TIME OF SEAL GLAND INSTALLATION SHALL NOT BE LESS THAN 1/2". IF THE JOINT OPENING IS LESS, INSTALLATION SHALL BE POSTPONED UNTIL THE TEMPERATURE DROPS A SUFFICIENT AMOUNT TO ALLOW THE MINIMUM 1/2" OPENING.

**EXAMPLE**

**GIVEN** - DISTANCE FROM THE CENTERLINE OF JOINT TO THERMAL NEUTRAL POINT OF SUPERSTRUCTURE ALONG CENTERLINE OF ROADWAY IS 287.5'; SKEW ANGLE OF THE EXPANSION JOINT IS 30°; ANTICIPATED AMBIENT TEMPERATURE AT TIME OF JOINT INSTALLATION IS 65° F.

**FIND** - REQUIRED STRIP SEAL GLAND SIZE AND JOINT OPENING (DIMENSION "A") AT TIME OF JOINT ARMOR INSTALLATION.

**SOLUTION**

- (A) ENTER TABLE "A" AT ① WITH 287.5' AND FIND THAT THE REQUIRED STRIP SEAL GLAND SIZE IS 4 INCHES.
- (B) ENTER TABLE "C" AT ② WITH 287.5' x COSINE OF 30°=248.98' AND FIND REQUIRED JOINT OPENING AT 65° F. IS 1.86".

NOTE: STEP (B) REQUIRED ONLY AT TIME OF CONSTRUCTION.

TABLE E (PHYSICAL PROPERTIES OF SEAL ELEMENT)		
PROPERTY	REQUIREMENT	ASTM METHOD
TENSILE STRENGTH, MIN. PSI	2000	D412
ELONGATION @ BREAK, MIN. PERCENT	250	D412
HARDNESS, TYPE A DUROMETER, POINTS	60 ± 5	D2240 (MODIFIED)
OVEN AGING, 70 HR @ 212° F TENSILE STRENGTH, LOSS, MAX. ELONGATION, LOSS, MAX.	20 PERCENT 20 PERCENT	D573
HARDNESS, TYPE A DUROMETER, POINTS CHANGE	0 TO +10	D2240 (MODIFIED)
OIL SWELL, ASTM OIL 3 70 HR @ 212° F, WEIGHT CHANGE MAX	45 PERCENT	D471
OZONE RESISTANCE 20 PERCENT STRAIN, 300 PPM IN AIR, 70 HR @ 104° F (WIPED WITH TOLUENE TO REMOVE SURFACE CONTAMINATION)	NO CRACKS	D1149
LOW TEMPERATURE STIFFENING 7 DAYS @ 14° F HARDNESS, TYPE A DUROMETER, POINTS CHANGE	0 TO +15	D2240 (MODIFIED)
COMPRESSION SET, 70 HR @ 212° F MAX.	40 PERCENT	D395 METHOD B