

ITEM 516 SEMI-INTEGRAL ABUTMENT EXPANSION JOINT SEAL, AS PER PLAN

INSTALL A 3 FOOT WIDE NEOPRENE SHEET AT LOCATIONS SHOWN IN THE PLANS. SECURE THE NEOPRENE SHEETING TO THE CONCRETE WITH 1-1/4" X #10 GAGE (LENGTH X SHANK DIAMETER) GALVANIZED BUTTON HEAD SPIKES THROUGH A 1 INCH OUTSIDE DIAMETER, #10 GAGE GALVANIZED WASHER. MAXIMUM FASTENER SPACING IS 9 INCHES. USE OF OTHER SIMILAR GALVANIZED DEVICES, WHICH WILL NOT DAMAGE EITHER THE NEOPRENE OR THE CONCRETE WILL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.

CENTER THE NEOPRENE STRIPS ON ALL JOINTS. FOR HORIZONTAL JOINTS, SECURE THE HORIZONTAL NEOPRENE STRIP BY USING A SINGLE LINE OF FASTENERS, STARTING AT 6 INCHES, +/-, FROM THE TOP OF THE NEOPRENE STRIP. FOR THE VERTICAL JOINTS SECURE THE VERTICAL NEOPRENE STRIP BY USING A SINGLE VERTICAL LINE OF FASTENERS, STARTING AT 6 INCHES, +/-, FROM THE VERTICAL EDGE OF THE NEOPRENE STRIP NEAREST TO THE CENTERLINE OF ROADWAY. FOR VERTICAL JOINTS, INSTALL 2 ADDITIONAL FASTENERS AT 6 INCHES, CENTER TO CENTER, ACROSS THE TOP OF THE NEOPRENE STRIP ON THE SAME SIDE OF THE VERTICAL JOINT AS THE SINGLE VERTICAL ROW OF FASTENERS IS LOCATED.

THE VERTICAL NEOPRENE STRIPS SHALL COMPLETELY OVERLAP THE HORIZONTAL STRIPS. LAP LENGTHS OF THE HORIZONTAL STRIPS THAT ARE NOT VULCANIZED OR ADHESIVE BONDED, SHALL BE AT LEAST 1 FOOT IN LENGTH, OR 6 INCHES IN LENGTH IF THE LAP IS VULCANIZED OR ADHESIVE BONDED. NO LAPS ARE ACCEPTABLE IN VERTICALLY INSTALLED NEOPRENE STRIPS.

THE NEOPRENE SHEETING SHALL BE 3/32" THICK GENERAL PURPOSE, HEAVY DUTY NEOPRENE SHEET WITH NYLON FABRIC REINFORCEMENT. THE SHEETING SHALL BE "FAIRPRENE NUMBER NN-0003", BY E. I. DUPONT DE NEMOURS AND COMPANY, INC., "WINGPRENE" BY THE GOODYEAR TIRE AND RUBBER COMPANY, OR AN APPROVED ALTERNATE. THE NEOPRENE SHEETING SHALL CONFORM TO THE FOLLOWING:

DESCRIPTION OF TEST	ASTM METHOD	REQUIREMENT
THICKNESS, INCHES	D751	0.094 +/- 0.01
BREAKING STRENGTH, GRAB, LBS, MINIMUM (LONG. X TRANS.)	D751	700 X 700
ADHESIVE STRIP, 1" WIDE X 2" LONG, LBS MINIMUM	D751	9
BURST STRENGTH, PSI MINIMUM	D751	1400
HEAT AGING, 70 HR, 212 DEGREES F, 180 DEGREES BEND WITHOUT CRACKING	D2136	NO CRACKING OF COATING
LOW TEMP. BRITTLENESS, 1 HR, -40 DEGREES F, BEND AROUND 1/4" MANDREL	D2136	NO CRACKING OF COATING

METHOD OF MEASUREMENT: THE DEPARTMENT WILL MEASURE THE TOTAL LENGTH OF JOINT TO BE SEALED BY THE NUMBER OF FEET.

BASIS OF PAYMENT: THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT PRICE FOR ITEM 516, SEMI-INTEGRAL ABUTMENT EXPANSION JOINT SEAL, AS PER PLAN.

ITEM 516, JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN

THIS WORK CONSISTS OF RAISING OR REPOSITIONING EXISTING STRUCTURES TO THE DIMENSIONS AND REQUIREMENTS DEFINED IN THE PROJECT PLANS.

SUBMIT CONSTRUCTION PLANS IN ACCORDANCE WITH CMS 501.05.

IF, DURING THE JACKING OPERATIONS, DAMAGE TO THE STRUCTURE IS VISUALLY OBSERVED, IMMEDIATELY CEASE THE JACKING OPERATION AND INSTALL SUPPORTS TO THE SATISFACTION OF THE ENGINEER. ANALYZE THE DAMAGE AND SUBMIT A METHOD OF CORRECTION TO THE ENGINEER FOR APPROVAL. THE DEPARTMENT WILL NOT PAY FOR THE COST OF REQUIRED REPAIRS. THE BRIDGE BEARINGS SHALL BE FULLY SEATED AT ALL CONTACT AREAS. IF FULL SEATING IS NOT ATTAINED, SUBMIT A REPAIR PLAN TO THE ENGINEER. THE DEPARTMENT WILL NOT PAY FOR THE REPAIR COSTS TO ENSURE FULL SEATING ON BEARINGS.

THE DEPARTMENT WILL MEASURE THIS WORK ON A LUMP SUM BASIS.

THE DEPARTMENT WILL PAY FOR THE ACCEPTED QUANTITIES AT THE CONTRACT PRICE FOR ITEM 516, JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN.

PORTABLE CONCRETE BARRIER:

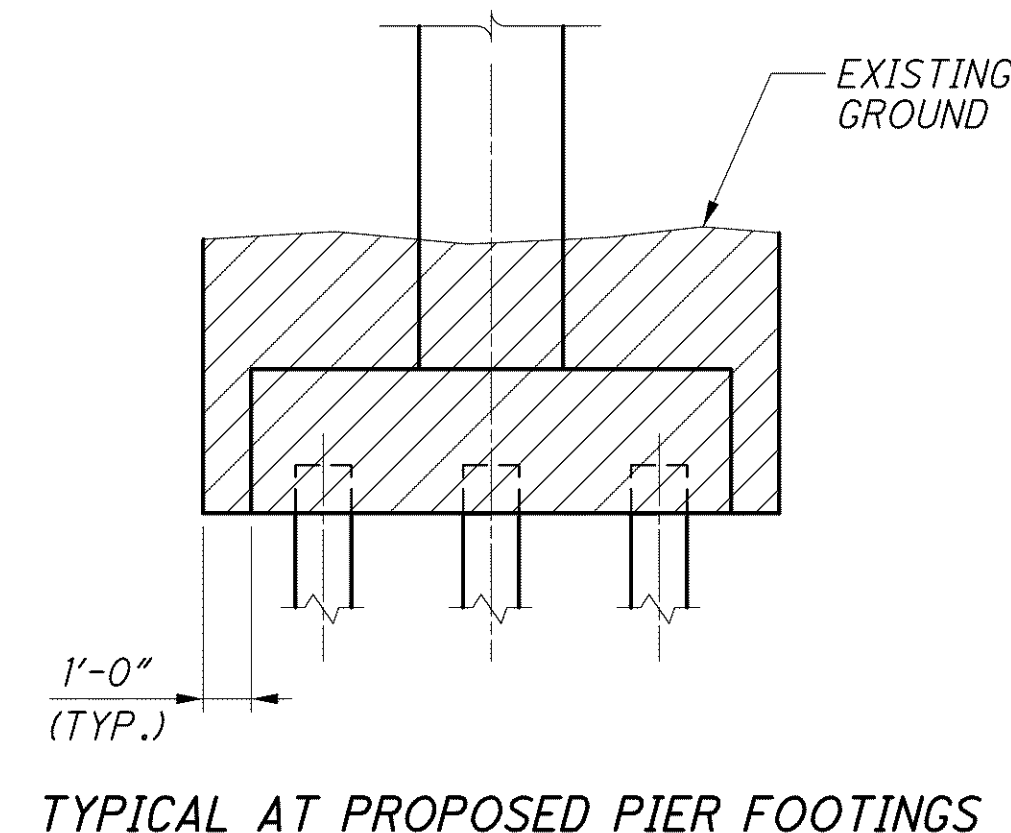
FURNISHING, INSTALLING, MAINTAINING AND REMOVING PORTABLE BARRIER ON THE BRIDGE SHALL BE INCLUDED IN ITEM 614 OF THE ROADWAY QUANTITIES FOR PAYMENT. SEE MOT PLANS FOR DETAILS AND PAYMENT OF TEMPORARY DRUMS.

INSPECTION OF EXISTING STRUCTURAL STEEL

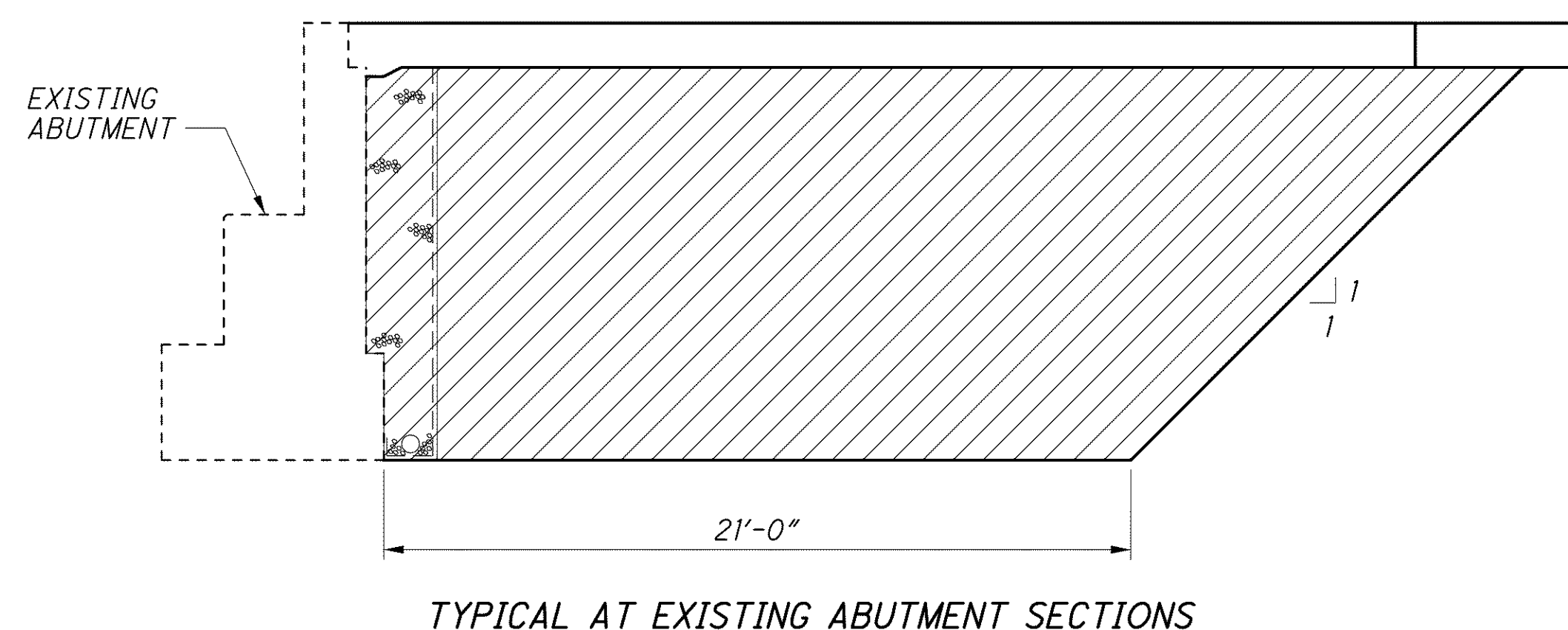
THE ENGINEER WILL VISUALLY INSPECT ALL EXISTING BUTT-WELDED SPLICES AND/OR TOP FLANGE COVER PLATE FILLET WELDS TO ENSURE THE WELDS, PLATES AND BEAMS OR GIRDERS ARE FREE OF DEFECTS AND CRACKS. IF NECESSARY, REMOVE ALL DECK SLAB HAUNCH FORMS IMMEDIATELY ADJACENT TO SUCH WELDS THAT MAY INTERFERE WITH THE ENGINEER'S INSPECTION. THE INSPECTION WILL NOT TAKE PLACE UNTIL THE TOP FLANGES ARE CLEANED ACCORDING TO 511.10, BUT IT WILL BE DONE BEFORE THE DECK SLAB REINFORCEMENT IS INSTALLED. THE DEPARTMENT WILL PAY FOR THE COST ASSOCIATED WITH THIS INSPECTION WITH ITEM 511, CLASS HP CONCRETE, BRIDGE DECK, AS PER PLAN. THE ENGINEER WILL REPORT ALL CRACKS FOUND TO THE OFFICE OF CONSTRUCTION ADMINISTRATION, BRIDGE CONSTRUCTION SPECIALIST, ALONG WITH SPECIFIC INFORMATION ON LOCATION OF THE CRACKS, LENGTH, AND DEPTH SO AN EVALUATION AND REPAIR OR REPLACEMENT RECOMMENDATION CAN BE MADE.

ABBREVIATIONS:

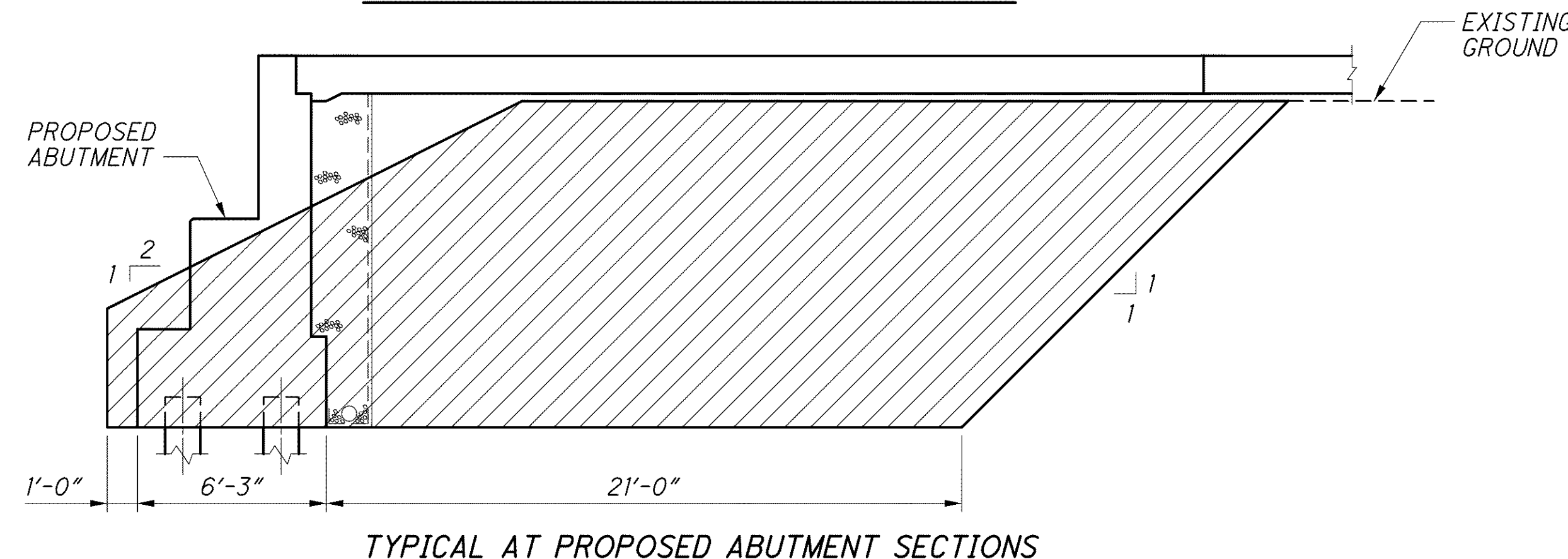
- N.F. = NEAR FACE
- F.F. = FAR FACE
- E.F. = EACH FACE
- TYP. = TYPICAL
- MIN. = MINIMUM
- STA. = STATION
- SPA. = SPACES
- CONST. = CONSTRUCTION
- EL. = ELEVATION
- C.I.P. = CAST-IN-PLACE
- BRG. = BEARING
- EX. = EXISTING
- PROP. = PROPOSED
- A.P.P. = AS PER PLAN
- R.A. = REAR ABUTMENT
- F.A. = FORWARD ABUTMENT
- O/O = OUT TO OUT
- CLR. = CLEAR
- LT. = LEFT
- RT. = RIGHT
- EST. = ESTIMATE
- TBR = TO BE RELOCATED
- R = REMOVE
- TR = TO REMAIN
- M = MIDDLE
- P.E.J.F. = PREFORMED EXPANSION JOINT FILLER
- P.F.J. = PRECOMPRESSED FOAM JOINT
- P.C.P.P. = PERFORATED CORRUGATED PLASTIC PIPE
- N.P.C.P.P. = NON-PERFORATED CORRUGATED PLASTIC PIPE
- INV. = INVERT
- FWD. = FORWARD
- ABUT. = ABUTMENT
- CONC. = CONCRETE
- EA. = EACH
- STD. = STANDARD
- DWG. = DRAWING
- DIA. = DIAMETER
- E.B. = EASTBOUND
- W.B. = WESTBOUND
- W.P. = WORK POINT
- C/C = CENTER TO CENTER
- STRUCT. = STRUCTURE
- TEMP. = TEMPORARY
- C.J. = CONSTRUCTION JOINT
- UNO = UNLESS NOTED OTHERWISE
- LLV = LONG LEG VERTICAL
- T = TOP
- B = BOTTOM



TYPICAL AT PROPOSED PIER FOOTINGS



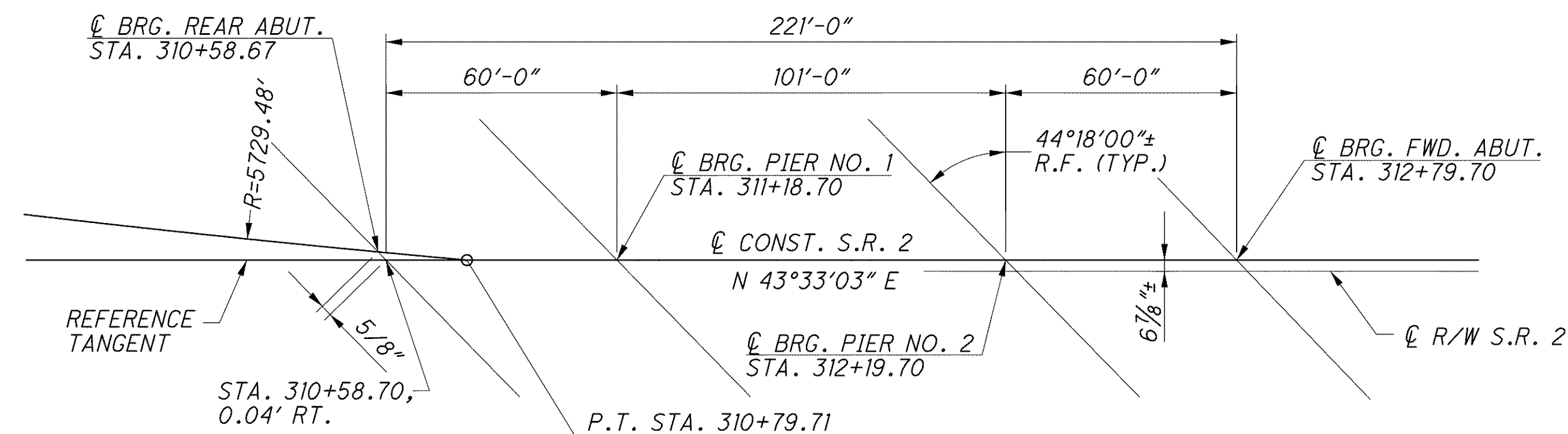
TYPICAL AT EXISTING ABUTMENT SECTIONS



TYPICAL AT PROPOSED ABUTMENT SECTIONS

EXCAVATION LIMITS

LIMITS OF UNCLASSIFIED EXCAVATION, AS PER PLAN



REFERENCE TANGENT DIAGRAM

DESIGN AGENCY: PARSONS BRINCKERHOFF QUAADE & DOUGLAS, INC. 614 W. SUPERIOR AVE., SUITE 400 CLEVELAND, OHIO 44113

DATE: 6-19-08
 RJO
 STRUCTURE FILE NUMBER: 4300572(L) 4300602(R)

DESIGNED: BMG
 CHECKED: PWP

GENERAL NOTES
 BRIDGE NO. LAK-2-0400 L&R
 STATE ROUTE 2 OVER VINE STREET

LAK-2-3.32
 PID No. 13486

4 / 67

1330
 1679