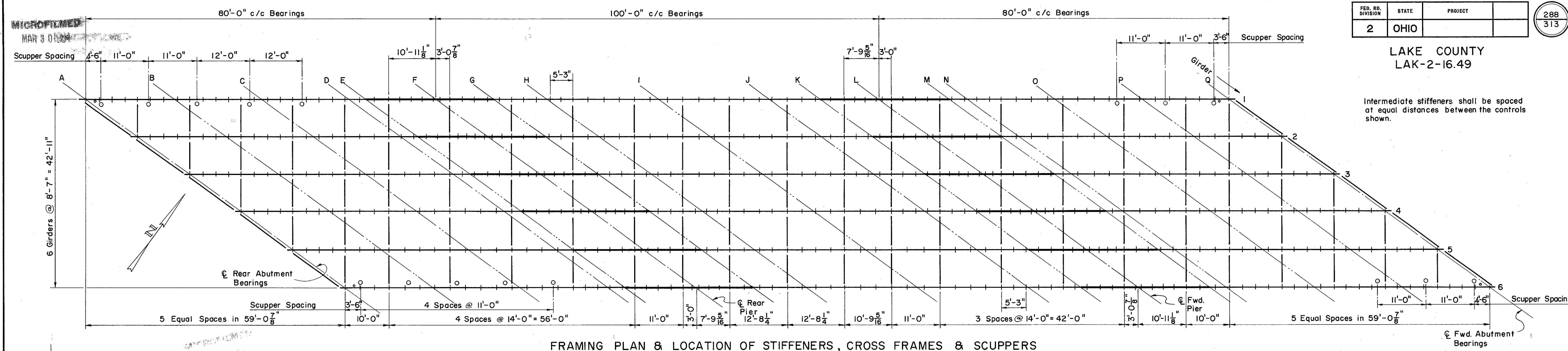
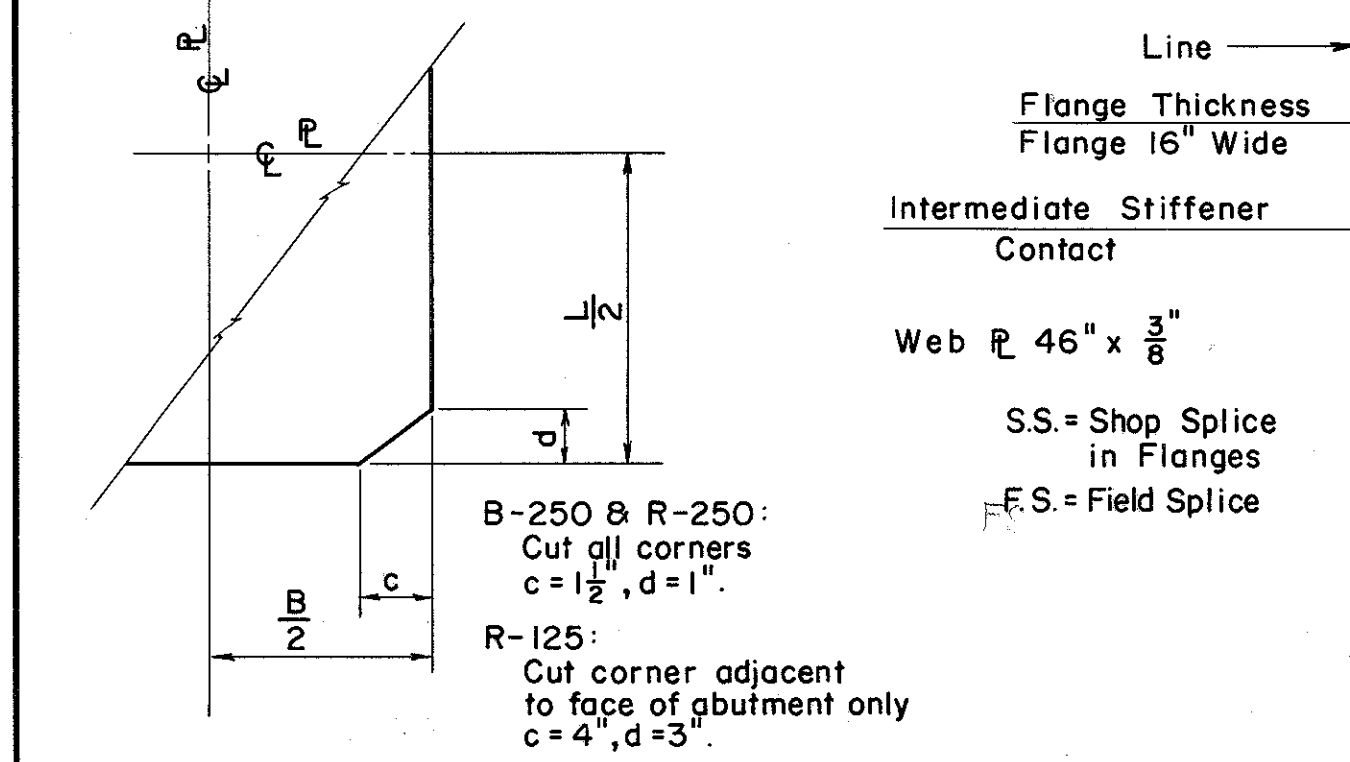


LAKE COUNTY
LAK-2-16.49

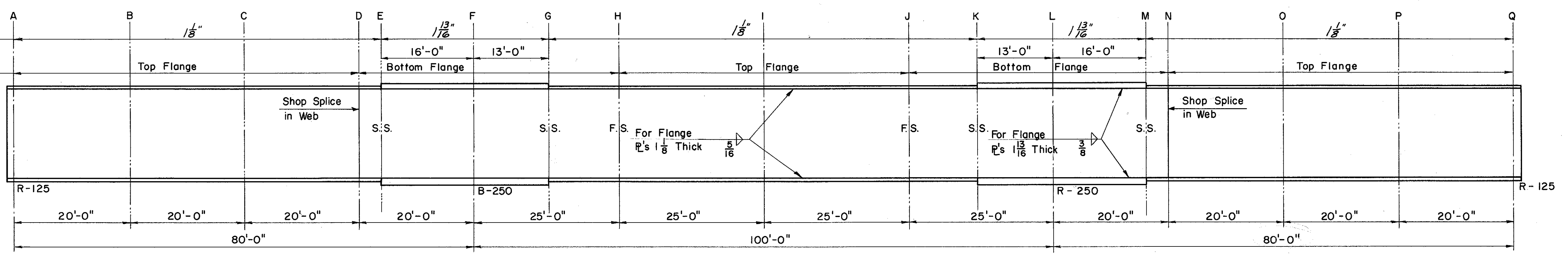
Intermediate stiffeners shall be spaced at equal distances between the controls shown.



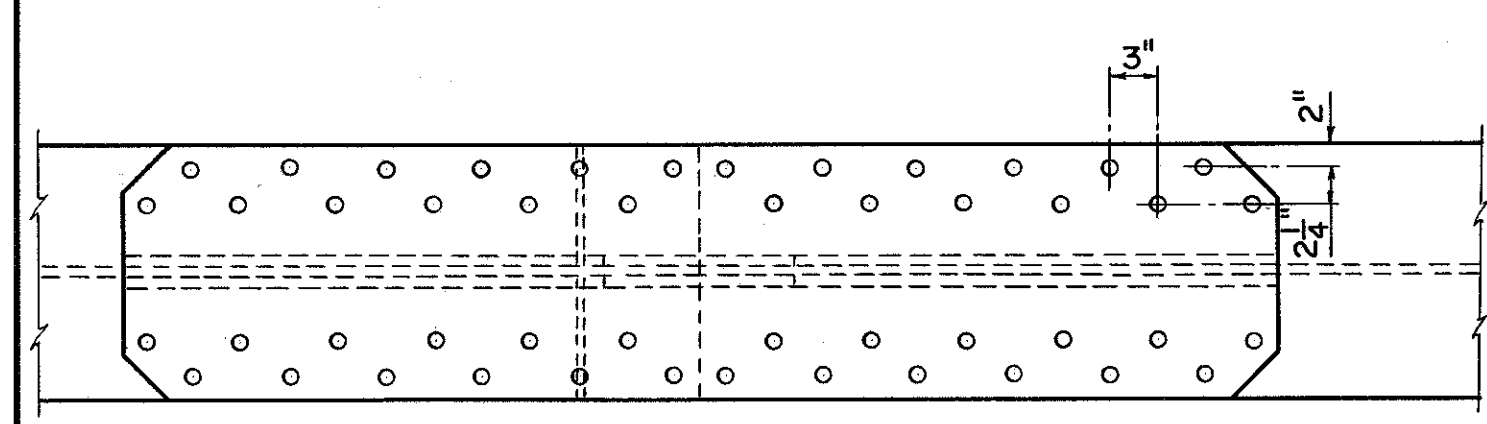
FRAMING PLAN & LOCATION OF STIFFENERS, CROSS FRAMES & SCUPPERS



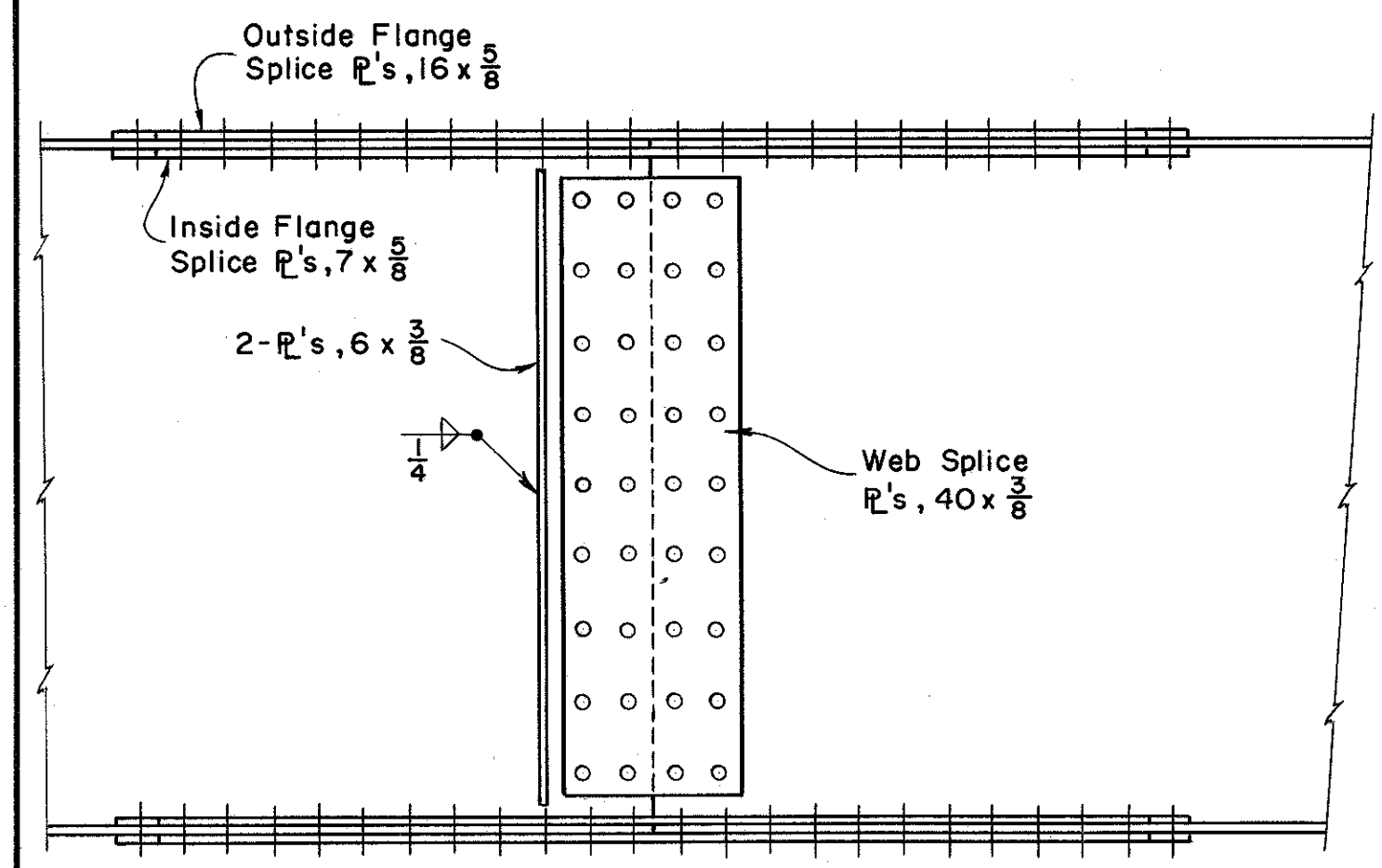
MODIFICATION OF MASONRY PLATES



GIRDER DETAIL

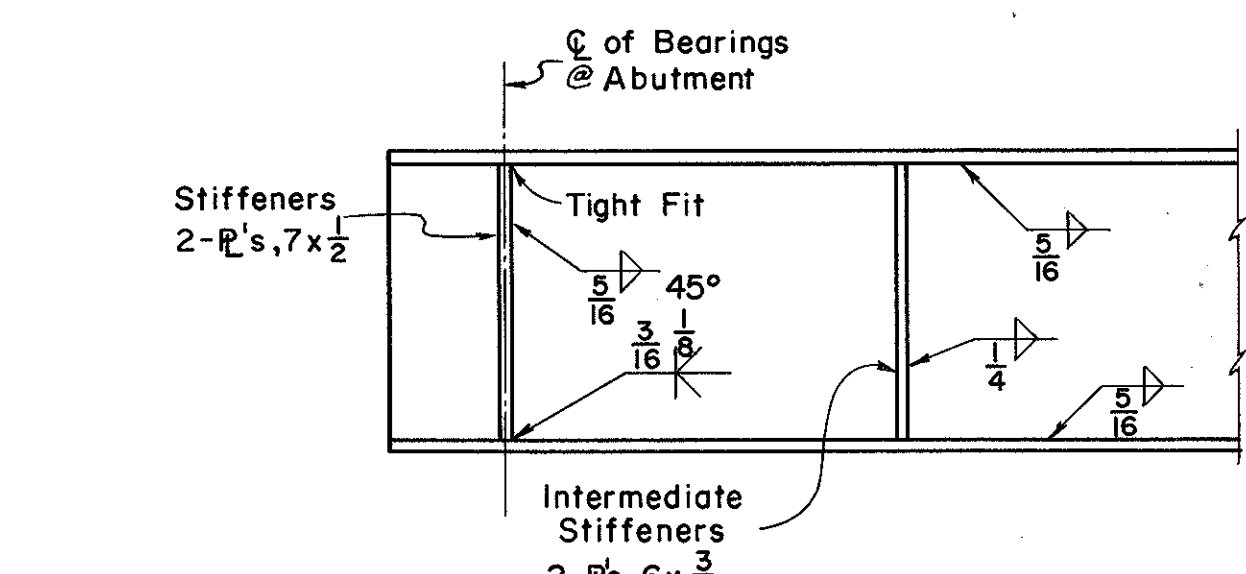


PLAN

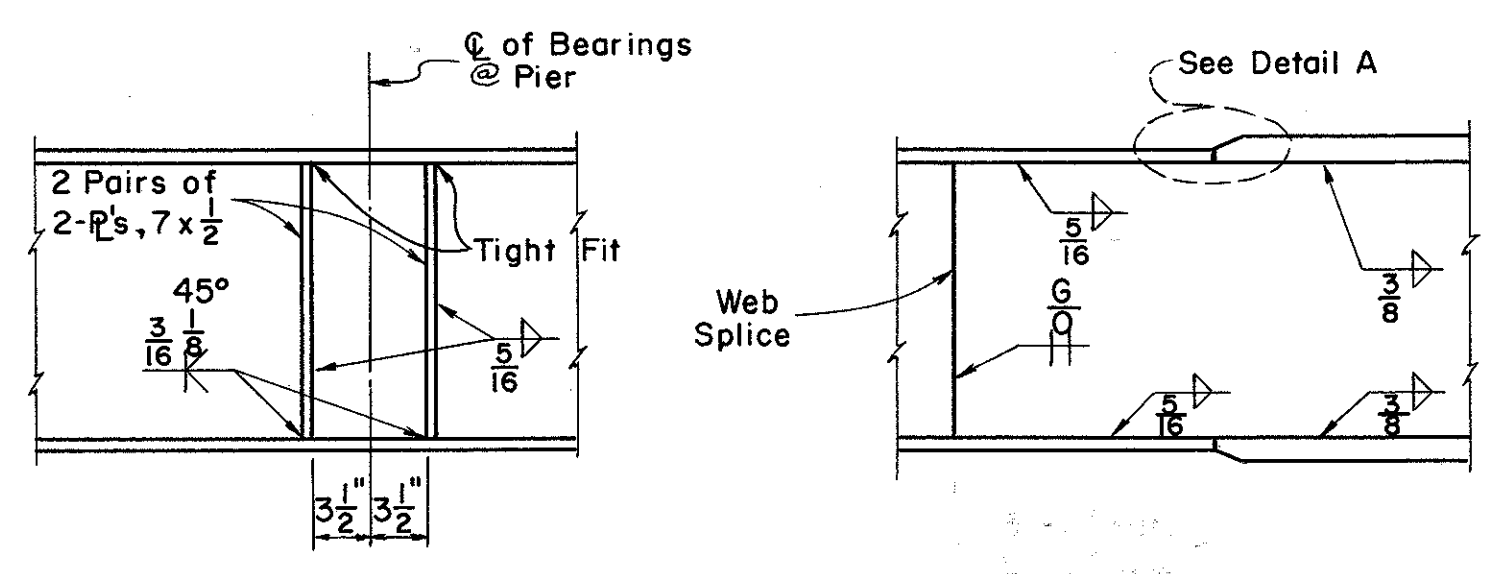


ELEVATION

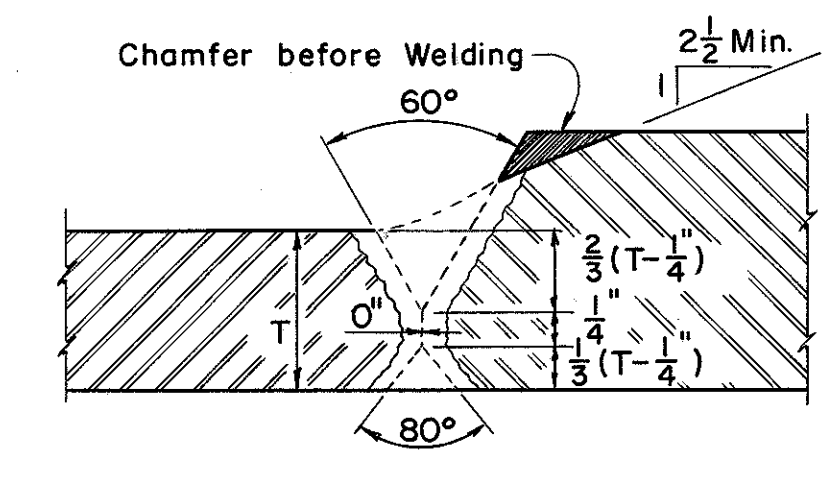
FIELD SPLICE DETAILS



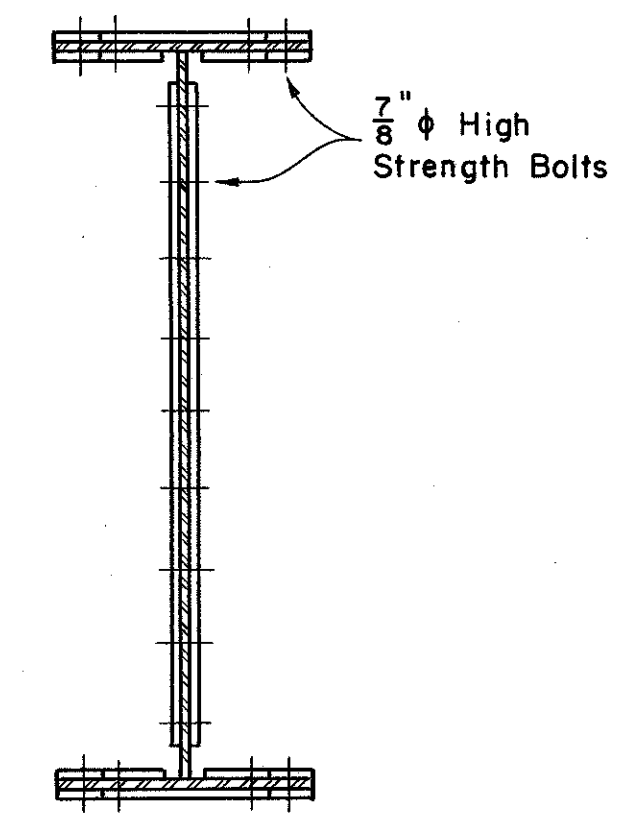
STIFFENER DETAILS



SHOP SPLICE DETAILS



DETAIL A



SECTION

DEFLECTION & CAMBER DATA									
LINE	B	C	D	H	I	J	N	O	P
Deflection due to weight of steel each girder	1/8"	1/8"	0	0	1/8"	0	0	1/8"	1/8"
Deflection due to remaining dead load, each girder	1/2"	5/8"	3/8"	1/4"	5/8"	1/4"	0 1/4"	5/8"	1/2"
Convexity required for vertical curve	1/8"	1/4"	1/4"	5/8"	7/8"	5/8"	0 1/4"	5/8"	5/8"
Sum of deflection and convexity	3/4"	1"	5/8"	7/8"	1 5/8"	7/8"	4/8"	1 3/8"	1"
Required Camber	3/4"	1"	5/8"	7/8"	1 5/8"	7/8"	4/8"	1 3/8"	1"

NOTES:
 REFERENCE shall be made to Standard Drawing RB-1-55 Revised 2-2-59 for details of bearings. Cut corners of masonry plates in accordance with detail shown on this sheet.
 WELDING of structural steel shall be class A.
 ALL BUTT WELDS shall be radiographically inspected according to supplemental specifications S-307 dated 8/23/62.
 SHOP SPLICES: If additional shop splices are necessary, their location and detail shall be submitted to the Director for approval prior to ordering of material.
 BUTT WELDS shall be formed by welding from both sides with back gouging before welding from second side.

PREPARED BY
CAPITOL ENGINEERING ASSOCIATES, DILLSBURG, PA. FOR

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
DIVISION OF DESIGN AND CONSTRUCTION
BUREAU OF BRIDGES

FRAMING PLAN & GIRDER DETAILS
BRIDGE NO. LAK-2-1862
RELOC. S.R. 2 UNDER RECONSTR. U.S. 20
LAKE COUNTY
STA. 99 + 73.07

DESIGNED M.C.P. L.L.D.	DRAWN M.J.F.	TRACED FWB	CHECKED L.L.D. M.J.F.	REVIEWED DATE G.S.W. M.C.P.	REVISED
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