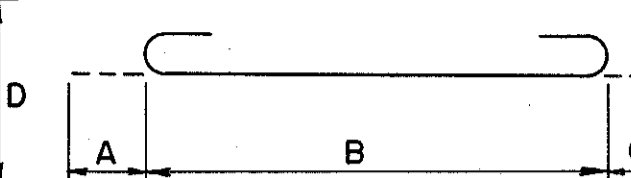
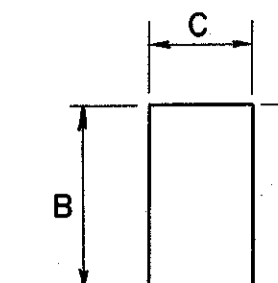
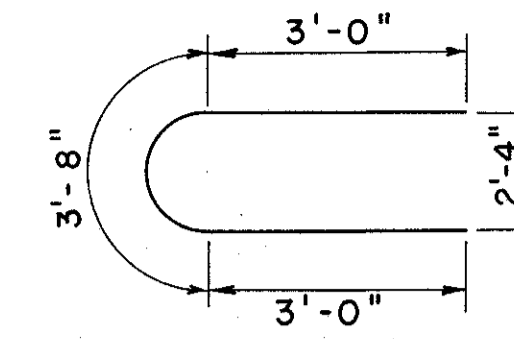
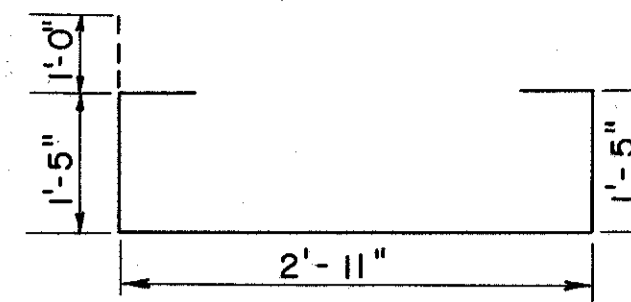
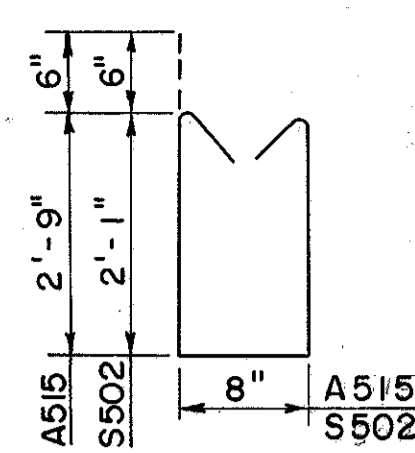
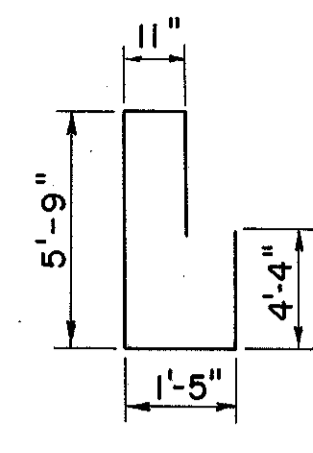
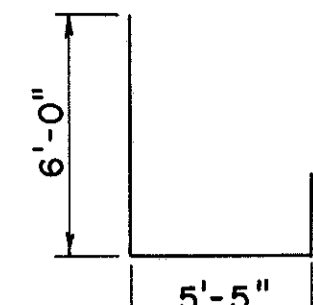
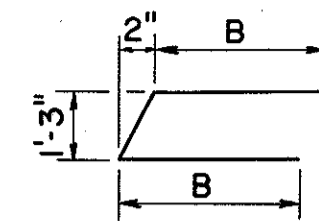
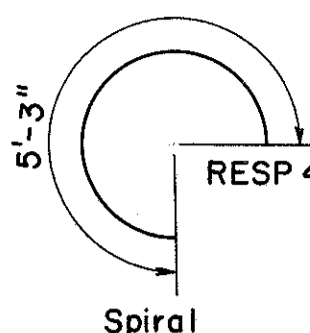


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FED. RD. DIVISION	STATE	PROJECT	244 313
2	OHIO	LAK-2-16.49	

LAKE COUNTY
LAK-2-16.49



REINFORCING STEEL LIST

PIERS											ABUTMENTS											SUPERSTRUCTURE														
MARK	REAR PIER	CENTER PIER	FWD. PIER		TOTAL	LENGTH	WEIGHT	TYPE	A	B	C	D	G	MARK	REQ'D REAR	REQ'D FWD.	TOTAL	LENGTH	WEIGHT	TYPE	B	C	D	VARIABLE BY	NO. OF EACH	MARK	TOTAL	LENGTH	WEIGHT	TYPE	B	C	D	VARIABLE BY	NO. OF EACH	
P1001	18	18	18		54	44'-0"	10,224	S						A801	14	14	28	21'-6"	1,607	S						S701	285	36'-2"	21,069	S						
P1002	4	4	4		12	34'-0"	1,756	S						A802	2	2	4	10'-9"	115	S						S701A	2628	35'-2" to 9'-2"	4,269	S	1178				2'-2" 2	
P1003	3	3	3		9	15'-0"	581	S						A803	6	4	10	11'-3"	300	S						S701B	10	8'-3"	169	S						
P1004	6	6	6		18	10'-6"	813	S						A804	4	6	10	11'-6"	307	S						S601	285	36'-2"	15,483	S						
P1005	4	4	4		12	30'-0"	1,549	S						A601	25	25	50	12'-8"	951	-						S601A	2628	35'-2" to 9'-2"	933	S	866				2'-2" 2	
P901	36	36	36		108	5'-5"	1,990	I		5'-0"	8"	0		A602	37	37	74	14'-0"	1,556	-						S601B	10	8'-3"	124	S						
P902	36	36	-		72	18'-0"	4,406	S						A603	32	32	64	6'-0"	577	I	1'-7"	3'-2"	1'-7"			S602	549	27'-9"	22,883	S						
P902A	-	-	36		36	21'-0"	2,570	S						A501	25	25	50	8'-4"	434	I	1'-7"	5'-5"	1'-7"			S603	72	26'-6"	2,866	S						
P903	4	4	4		12	9'-8"	394	-						A502	25	25	50	6'-9"	352	I	6'-0"	10"	0			S501	304	3'-10"	1,214	I	7"	2'-11"	7"			
P904	8	8	8		24	8'-6"	694	I		3'-0"	5'-9"	0		A503	25	25	50	10'-6"	546	I	3'-8"	3'-5"	3'-8"			S502	304	5'-7"	1,769	-						
P501	90	90	90		270	5'-8"	1,597	S						A504	40	40	80	20'-0"	1,669	S						S503	304	7'-3"	2,299	-						
P502	44	44	44		132	7'-5"	1,022	I		2'-6"	2'-8"	2'-6"		A505	38	36	74	21'-5" to 10'-8"	824	I	10'-3"	7"	0													
P503	2	2	2		6	33'-0"	206	S						A506	10	10	20	2'-6"	52	S																
														A507	20	-	20	8'-6"	177	S																
														A507A	-	10	10	8'-3"	86	S																
														A507B	-	10	10	8'-9"	91	S																
														A508	30	30	60	4'-5"	277	S																
														A509	22	24	46	3'-6"	168	S																
														A510	2	4	6	3'-9"	23	S																
														A511	12	12	24	4'-0"	102	S																
														A512	4	4	8	6'-6"	54	S																
														A513	8	4	12	11'-9"	147	S																
														A513A	-	4	4	12'-6"	52	S																
														A514	8	8	16	5'-0" to 3'-0"	67	3	2'-0" to 1'-0"				4" 4											
														A514A	10	10	20	6'-8" to 5'-4"	125	3	2'-0" to 2'-2"				2" 5											
														A514B	6	9	15	6'-10"	107	3	2'-11"															
														A515	24	27	51	6'-11"	368	-																
														A516	2	2	4	10'-9"	45	S																
														A516A	4	-	4	11'-6"	48	S																
														A516B	4	4	8	11'-10"	99	S																
														A516C	-	4	4	12'-9"	53	S																
														A516D	2	2	4	13'-4"	56	S																

SPIRALS

MARK	REAR PIER	CENTER PIER	FWD. PIER		TOTAL	LENGTH	WEIGHT	PITCH	NO. TURNS	CORE DIA.
Sp401	3	3	-		6	14'-9"	1,663	4 1/2"	42	32"
Sp401A	-	-	3		3	17'-10"	1,010	4 1/2"	51	32"

RAILING

MARK	TOTAL	LENGTH	WEIGHT	TYPE
R501	96	16'-10"		S
R501A	16	13'-6"		S
R502	4	11'-4"		S
R502A	8	11'-8"		S
R502B	4	12'-7"		S

Included with item S-14 for payment

REPLACEMENT BARS

MARK	NO.	LENGTH	WEIGHT	TYPE
RE 1000	1	7'-3"		S
RE 900	1	6'-10"		S
RE 800	1	6'-6"		S
RE 700	1	6'-3"		S
RE 600	1	5'-11"		S
RE 500	1	5'-7"		S
RESP 4	1	5'-3"		Bent

NOTES

Bar Size is indicated in the bar mark. The first digit where three digits are used, and the first two digits where four digits are used, indicate the size of the bar. For example A700 is a No. 7 size bar and A1014 is a No. 10 size bar.

All dimensions are out to out of the bar.

Type "S" is straight bar.

The "length" shown in steel list for the spiral bars is the distance from the top of the footing to the bottom of cap.

The "number of turns" shown in the steel list for the spiral bars is the "length" divided by the pitch, plus three (3) turns (total number of closed coils) expressed as the nearest whole number.

Spiral reinforcing bars shall not have deformations but shall in other respects conform to item S-4.

1/2 closed coils shall be provided at the ends of each spiral unit.

Four steel channel, tee or angle spacers, weight approximately 0.68 pounds per linear foot of spacer, shall be provided for each spiral unit. They shall be equally spaced along the periphery of the coil. The number of pounds of these spacers, based on 0.68 pounds per linear foot will be paid for as reinforcing steel and is included in the tabulated quantity of spiral bars.

PREPARED BY
CAPITOL ENGINEERING ASSOCIATES, DILLSBURG, PA.
FOR

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
DIVISION OF DESIGN AND CONSTRUCTION
BUREAU OF BRIDGES

REINFORCING BAR LIST
BRIDGE NO. LAK-2-1673
RELOC. S.R. 2 UNDER RELOC. MANTLE ROAD
LAKE COUNTY
STA. 783 + 61.13

DESIGNED	DRAWN	TRACED	CHECKED	REVIEWED DATE	REVISED
M.J.F.	M.J.F.	FWB	L.L.D.	G.S.W.	11-20-64
L.L.D.			M.J.F.	M.C.P.	